



Donegal County Council



Donegal County Council

Planning and Development Regulations 2001 (as amended)
Nature and Extent of a proposed development in accordance with Part VIII of the
above

**Donegal Municipal District Active Travel Project
Bundoran to Ballyshannon, Co. Donegal**

PROJECT NAME: Donegal Municipal District Active Travel Projects

REPORT NAME: Scheme 8 – Bundoran to Ballyshannon Part 8 Planning Report

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Revision	Description	Author:	Date	Reviewed By:	Date	Authorised by:	Date
A	Issue	RF	23/07/23	JOF	26/07/23	MMcD	26/07/23

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[Appendix E – Planning Notice](#)



1.0 EXECUTIVE SUMMARY

This report relates to the proposed R267 Bundoran to Ballyshannon Active Travel Project. The project will provide a cycle and pedestrian corridor between two towns of Bundoran and Ballyshannon which will be reserved exclusively for non-motorised journeys, enhancing both the environment and the quality of life of the surrounding area. The route is enhanced as the landscape between Bundoran and Ballyshannon is particularly scenic.

The route will meet the satisfactory standards of width, gradient and surface condition to ensure they are both user friendly and low risk for users of all abilities.

This proposed scheme will be located on the northern side of the R267 consist of 3m wide shared cycle and pedestrian route commencing 50m west of the Bundoran Roundabout travelling East along a 3.5km rural route. On the approach to Ballyshannon at the speed limits the shared area separates into a 2m wide pedestrian walkway and the 3m wide 2-way cycle track to the Ballyshannon roundabout.

This scheme proposes the narrowing of the R267 carriageway;

- in the urban area to DMURS standards with the removal of central medians and edge of road hatching and;
- in the rural area to a Type 3 single carriageway in accordance with rural road link design from the TII.

This proposed works are to facilitate the cycle and pedestrian route which in turn will reduce the speed on this road. This will provide an opportunity for modal shift from motorised vehicles to more sustainable modes of transport.

The report provides a background to the proposed development, the nature and extent of the works being proposed and key features of the project.

The report is prepared in accordance with Part 8 of the Planning & Development Regulations 2001 (as amended).

2.0 INTRODUCTION

2.1 PROJECT BACKGROUND AND NEED FOR THE PROJECT

Bundoran is a seaside town, located at the southern periphery of County Donegal. Tourism has been at the heart of the local economy since the 18th century. Ballyshannon is located at the mouth of the River Erne approximately 5km northeast of Bundoran and is one of the oldest towns in Ireland.

The extent of the scheme is defined by a roundabout at each end of the scheme on the outskirts of Bundoran and Ballyshannon. The R267 between Bundoran and Ballyshannon was previously part of the N15 National Road but was re-categorised when the Bundoran – Ballyshannon Bypass was constructed. This left a wide single carriageway road with no pedestrian or cyclist facilities, which was designed to provide priority to vehicular traffic.

Donegal County Council proposes to provide a cycle and pedestrian route between the towns of Bundoran and Ballyshannon. The proposal seeks to improve infrastructure and connectivity to provide secure active travel linkages between the population centres of Ballyshannon, Bundoran and surrounding environs. The Projects promote the 10-minute active travel journey from residential areas to key education, employment, commercial, recreational, retail and tourist destinations within Bundoran and Ballyshannon that works for all members of the community and promote modal shift.

The objectives of this project are as follows:

- It is an objective of Donegal County Council to directly enhance permeability for communities to access key education, employment, commercial, recreational, retail and tourist destinations within Bundoran/Ballyshannon and throughout County Donegal.
- It is an objective of Donegal County Council, to promote the 10-minute town active travel concept through the provision of a high-quality urban environment for people along the route.
- It is an objective of Donegal County Council, to promote the creation of ‘active environments’ is implemented across all areas of Bundoran/Ballyshannon, resulting in safe, accessible and sustainable movement and travel for all social groups which will help improve health outcomes and reduce inequalities.
- It is an objective of Donegal County Council that the new structural infrastructure conforms to design standards as defined by the Design Manual for Urban Roads and Streets (DMURS).
- It is an objective of Donegal County Council that the new road realignment meets minimum design standards for the appropriate Design Speed and supports sustainable transport systems as far as is practicable.
- It is an objective of Donegal County Council, that the integrity of Special Areas of Conversations and associated bays be maintained and possibly improved by the works.

2.2 PROJECT SPECIFIC AIMS AND OBJECTIVES

The route along the R267 from the Bundoran roundabout to Ballyshannon roundabout is approximately 5Kilometers in length and comprises of a rural and urban setting located within the scheme extent.

Approximately 3.35km of the route is within the 80kph speed limit. The area is a rural agricultural area with Finner Army camp to the north and a number of residential properties mainly to the south.

The remaining 1.65km is within the Urban section of Ballyshannon town with a speed limit of 60-50 Km/hr. On entering Ballyshannon from Bundoran there area number of business parks on the northern side, a Garda Station and some residential properties. However, the River Erne runs along the northern side of the R267 therefore few access points for business or residential properties.

The National Cycle Manual is the reference document used when designing the Bundoran/Ballyshannon Active Travel scheme. The quality of Service for both pedestrians and cyclists, the widths of the shared Cycle and Pedestrian route in the rural section and the 2-way cycle track within the urban section, along with principles of sustainable safety were taken in to account. The northern side of the R267 was identified for a number of reasons listed below:

- The R267 runs adjacent to Finner Camp where accesses are limited causing less conflict between vulnerable road users and vehicles.
- Views for the pedestrian and cyclist are greatly improved on the northern section giving views of the sea and the river Erne.
- It provides connectivity and uniformity throughout the entire.
- Limited access on the northern end within the 60/50 kph also.

The Rural Zone has a speed limit of 80kph but was designed for a speed of 100kph. The appropriate standards for the design of the rural section are TII Publications. The wide single carriageway cross section provided conforms to a Type 1 single carriageway in accordance with DN-GEO-03031 with associated hard shoulders and ghost island junctions. This cross section has a capacity for level of service D of 11,600 AADT. Given the current volume of traffic on the road, this is considerably over-designed and a Type 3 single carriageway in accordance with DN-GEO-03031 would be the appropriate cross section. The design speed of the road will be 80kph which the posted speed limit.

The Design Manual for Urban Roads and Streets (DMURS) is the reference document for road design in Ireland where a speed limit of 60Kph or less applies and will apply to the Urban section of the R267. The philosophy of DMURS is to encourage walking and cycling within town centres and outlying centres of population.

There are a few issues with the current situation at this location that are in contravention of DMURS:

- There is no continuity of footpath provision along the urban section of road.
- Road widths vary throughout the length of the R267 Assaroe Road.
- Pedestrian footways fail to comply with DMURS requirements.
- Lack of safe crossing locations

Figure 2-1 Site Extents and Location shows the extents of the proposed works.



Figure 2-1 Site Extents and Location

3.0 PLANNING POLICY DOCUMENTS, GUIDELINES AND LEGISLATION

The following sections describe a non-exhaustive list of European, National, Regional and Local Policy Documents and Guidelines which contain clear policy objectives to promote and develop sustainable transport and vulnerable road user provision.

3.1 EUROPEAN POLICY AND GUIDELINES

3.1.1 Europe 2020 – A Strategy for Smart, Sustainable and Inclusive Growth

This document puts forward three mutually reinforcing priorities for smart, sustainable, and inclusive growth. Sustainable transport strategy is set out under the “sustainable growth” priority, through flagship Initiative: “Resource efficient Europe”, which supports a shift towards a resource efficient and low carbon economy.

3.2 NATIONAL AND REGIONAL POLICY DOCUMENTS

3.2.1 Project Ireland 2040 National Planning Framework and National Development Plan 2018-2027

Project Ireland 2040 is the Irish Governments overarching policy initiative for the long-term planning of the State. It is informed by the Programme for a Partnership Government 2016, which recognises that economic and social progress go hand in hand and is made up of the “National Planning Framework to 2040” and the “National Development Plan 2018-2027”.

3.2.1 National Planning Framework to 2040

This is the Government’s high-level strategic plan for shaping the future growth and development of the country out to the year 2040. It seeks to achieve ten strategic outcomes including the following which are relevant to the Lifford:

- *National Strategic Outcome 4: Sustainable Mobility* including an objective to

“Develop a comprehensive network of safe cycling routes in metropolitan areas to address travel needs and to provide similar facilities in towns and villages where appropriate.”

- National Strategic Outcome 8: Transition to a Low-Carbon and Climate-Resilient Society including developing metropolitan cycling and walking networks and Greenways.
- National Policy Objective 46 includes the enhancement of “transport connectivity between Ireland and Northern Ireland, to include cross-border road and rail, cycling and walking routes, as well as blueways, Greenways and peatways.”

3.2.2 National Development Plan 2018 – 2027

The National Development Plan 2018–2027 is the most recent in the series of Government Capital plans adopted since 1988 and identifies the strategic priorities for public capital investment for all sectors to meet the strategic outcomes of the National Planning Framework.

It includes for investment in sustainable travel measures, including comprehensive Cycling and Walking Networks for metropolitan areas in delivering a transition to a Low-Carbon society.

3.2.3 Regional Planning Guidelines

The Regional Spatial and Economic Strategy produced by the Northern and Western Regional Assembly will support further investment in sustainable transport measures under its ‘Connected Ambition’ heading.

3.3 LOCAL POLICY DOCUMENTS

3.3.1 Donegal County Council Development Plan 2018 – 2024

- Transportation Objective T-O-13: To support the development of new walkways, walking routes, trains, greenways, and cycleways that maximise the potential for local, regional and all-island walking and cycling networks.
- Transportation Policy T-P-3: It is a policy of the Council to work in partnership with the Northern Ireland authorities to strengthen and improve existing cross border transportation links (including walking and cycling routes) to enable the targeted spatial and economic development of the Northwest City Region.
- Transportation Policy T-P-11: It is a policy of the Council to facilitate the appropriate development of affordable, multi-modal transport solutions that offer communities and future generations real transport choices such as park and ride; pedestrian and cycling; bus and taxi services; and ancillary infrastructure.
- Transportation Policy T-P-36: It is a policy of the Council to support and facilitate the maintenance, enhancement and expansion of the National Cycle Network.

- TOU-O-9: To support the development of new, and protect the functionality of existing, Greenways, walking and cycling routes as key components of an overall green tourism infrastructure and as standalone tourism products.

3.3.2 The Donegal Local Economic & Community Plan 2016 – 2022

- Section 1.9.3: To identify a programme of walkways, cycleways and Greenways within towns and their hinterland, to enhance town centre connectivity, support regeneration of town centres and improve health and recreation opportunities.
- Section 4.4.16: To maximise health and wellbeing outcomes for communities in the proposed development of the Northwest Greenway and other initiatives involving outdoorspaces.

3.4 RELEVANT LEGISLATION

Statutory approval for the scheme will be in accordance with the procedures identified in Part 8 of:

- The Planning and Development Act, 2000 (as amended).
- The Planning and Development Regulations, 2001 (as amended).

4.0 PROPOSED SCHEME

4.1 DESCRIPTION OF EXISTING INFRASTRUCTURE

4.1.1 Carriageways

The carriageway of the existing rural section was designed as a National Primary Route and had a primary function of serving a high volume of fast flowing traffic. The Bundoran to Ballyshannon Bypass removed this functionality but the nature and geometry of the road remained. The current situation still maintains a reasonably high volume of traffic given its use as a link between the towns and access to Finner Camp. Vehicle speeds remain high and would appear to be significantly higher than the posted 80kph speed limit.



Figure 4-1 Existing Carriageway Width

Transport Infrastructure Ireland have details of cross section requirements to provide for cycle tracks in this manner in their publication DN-GEO-03031 Rural Road Link Design and DN-GEO-03036 Cross Sections and Headroom.

		Desirable Min (m)	One Step Below Desirable Min (m)
One Way Cycle Facility	Low Volume	2.0	1.75
	High Volume	3.0	1.75
Two Way Cycle Facility	Low Volume	2.5	2.0
	High Volume	3.0	2.5
Shared Use One Way Cycle Facility with Pedestrians	Low Volume	3.0	2.0
	High Volume	4.0	3.0
Shared Use Two Way Cycle Facility with Pedestrians	Low Volume	3.0	2.0
	High Volume	5.0	3.0

Figure 4-2 DN GEO-03031 Extract 1

Road Type	Desirable Min (m)	One Step Below Desirable Min (m)
Type 2 and Type 3 Single Carriageway	2.0	1.5
Type 2 and Type 3 Dual Carriageway	6.0	3.0

Figure 4-3 DN-GEO-03036 Extract 2

The existing carriageway in the urban section has carriageway widths ranging from 8m to 12m, Junctions are wide and centreline hatching at varies locations.

The appropriate width band in DMURS is shown in *Figure 4-4 DMURS*. Given the nature of the road as a direct link between Bundoran and Ballyshannon and a main arterial route to Ballyshannon, the appropriate carriageway width would be between 6.5 and 7.0m in accordance with DMURS.

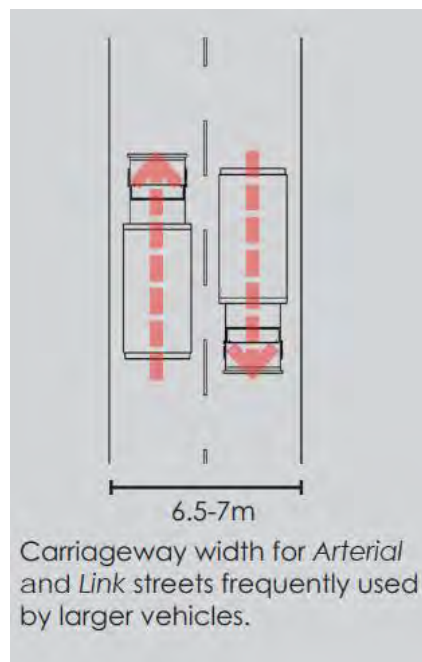


Figure 4-4 DMURS Extract

4.1.2 Footpaths

Footpaths do not exist along the length of the Rural section of the R267 with the except of 100m of a 1500mm footpath at the entrance to Finner camp.



Figure 4-5 Existing footpaths outside Finner camp on the R267

Footpaths commence at Saimer Drive junction on both sides of the R267 into the roundabout the extent of this study area. The footpaths vary in width with a average width of 2.1m. These footpaths fail to comply with DMURS requirements. The majority of footpaths are paving slabs or bituminous finish and are uneven.



Figure 4-6 Existing Footpaths

Footpaths should be designed free of obstacles and wide enough at a minimum to allow two wheelchairs to pass.

4.1.3 Crossing points

There is currently just one uncontrolled crossing point on R267 at Ballyshannon before the roundabout. Crossings are one of the most important aspects of street design as it is at this location that most interactions between pedestrians, cyclists and motor vehicles occur. Well designed and frequently provided crossings are critical to the balancing of movement priorities. The design of crossings, and the frequency at which they are provided, will have a significant impact on pedestrian/ cyclist mobility and comfort and the flow of vehicular traffic.



Figure 4-7 Crossing on R267 at Ballyshannon Roundabout

4.1.4 Public Transport

There is one Bus Stop facility within the study area, located on the rural section of this scheme opposite the entrance to Finner camp on the R267. The bus stop consists of a bus stop pole and is located on the hard shoulder of the south bound lane.

4.1.5 Public lighting

Public lighting exists along the urban section of this proposed scheme. New underground public lighting exists from the roundabout in Ballyshannon west for 400m these poles are located at the edge of the footpath on the roadside. The remainder of the public lighting has overhead cables and older lighting columns.

Public lighting does not exist on the rural section of the scheme.



Figure 4-8 Overhead lines and older Lighting columns

4.1.6 Drainage

There is no surface water drainage on the Rural section of this study area with the exception of some drainage associated with the existing footpath at the entrance to Finner Camp.

Surface water drainage provisions commence at the entrance to Ballyshannon from the speed limits and extend into the roundabout at the end of the study.

A formalised drainage design in accordance with Sustainable Urban Drainage Systems would be required at detailed design stage.

4.1.7 Landscaping

There are some verges along the R267 which require little to no maintenance.



Figure 4-9 Grass verges along the R267

4.2 NATURE AND EXTENT OF THE PROPOSED DEVELOPMENT

The principal feature of the proposed development is to provide a 5km Greenway along the R267 between Bundoran and Ballyshannon offering benefits for Safety to all road users, tourism, economic growth, culture and wellbeing.

A pavement renewal programme for existing road, undergrounding existing utility services, provisions of controlled and uncontrolled pedestrian crossings, reallocating of road space to pedestrian/Cyclists along the route.

Details are as follows:

4.2.1 Carriageways

The proposed carriageway dimensions of 3.25m each way is in accordance with the TII Publications for the rural section of 3.35Km and in accordance with DMURS for the 1.65km within the urban setting of 3.6m carriageway each way.

4.2.2 Footpaths

New footpath layout within the urban section from the speed limits into the roundabout in the town of Ballyshannon will be 2m with exception to a pinch point at chainage 4420 where the footpath and cycleway become a 3m shared section for 100m.

4.2.3 Cycleways

A new 3m shared cycle and pedestrian route with a grass verge separating the roadway and the new route extends from 50m west of the Finner Roundabout in Bundoran to the 60kph at the entrance to Ballyshannon.

4.2.4 Public Transport

The bus stop on the R 267 will be maintained at Finner Camp.

4.2.5 Lighting

New undergrounding of overhead lines on the urban section from the speed limit signs on the eastern side for approximately 730m followed by 380m on the western side. New lighting columns to be included throughout the urban section of the scheme.

4.2.6 Drainage

The proposed scheme requires some gullies to be relocated as the footpath will be extended into the existing carriageway.

4.2.7 Landscaping

Landscaping is included in the proposed development along some verges and junctions.

4.2.8 Road markings

Appropriate road markings in accordance with the Traffic Signs Manual will be used throughout the scheme where required to clearly identify shared areas, 2-way cycle tracks, carriageways, priorities and turning movements. Junction road markings will be upgraded to reflect the change in layout.

4.2.9 Signage

Where required, existing mainline directional and route confirmatory signage will be relocated or altered to accommodate the proposed cycle and pedestrian route.

New signage will be in accordance with the Traffic Signs Manual and erected along the cycle and pedestrian route to inform the users of exit and entry points, change of use etc.

In accordance with DMURS, signage will be kept to a minimum and it is not proposed to introduce further signage other than advanced warning of the pedestrian crossings.

4.2.10 Parking

Parking will not be affected by this proposed scheme.

4.2.11 Services

Localised relocation of service poles or pillars may be required where these are located along the route of the proposed footpath. All works will be agreed and carried out in accordance with the requirements of the appropriate utility providers. Overhead cables will also be undergrounded subject to agreement with the service providers.

4.3 DETAILS OF THE PROPOSED DEVELOPMENT

4.3.1 Drawings

The following Part VIII drawings of the proposed development are included in Appendix A.

Drawing Number	Drawing Title
11199-08-1000	Site Location
11199-08-1001	Existing layout Sheet 1 of 8
11199-08-1002	Existing layout Sheet 2 of 8
11199-08-1003	Existing layout Sheet 3 of 8
11199-08-1004	Existing layout Sheet 4 of 8
11199-08-1005	Existing layout Sheet 5 of 8
11199-08-1006	Existing layout Sheet 6 of 8
11199-08-1007	Existing layout Sheet 7 of 8
11199-08-1008	Existing layout Sheet 8 of 8
11199-08-1010	Preliminary Design Sheet 1 of 8
11199-08-1011	Preliminary Design Sheet 2 of 8
11199-08-1012	Preliminary Design Sheet 3 of 8
11199-08-1013	Preliminary Design Sheet 4 of 8
11199-08-1014	Preliminary Design Sheet 5 of 8
11199-08-1015	Preliminary Design Sheet 6 of 8
11199-08-1016	Preliminary Design Sheet 7 of 8
11199-08-1017	Preliminary Design Sheet 8 of 8
11199-08-1020	Typical Cross Sections
11199-08-1021	Typical Cross Sections

Table 4-1 List of Drawings

4.4 ROAD SAFETY AUDIT

As per TII publication TII-GE-STY-01024 (Road Safety Audit), a Stage 1 Safety Audit has been completed as part of this Report and is included in Appendix B. The Road Safety Audit was carried out by a qualified team of auditors to look at all aspects of road safety. All appropriate recommendations made by the audit team have been accepted by the designer and will be incorporated into the design for construction documents. The audit team is completely independent of the design process. A Stage 2 Safety Audit will then be carried out prior to the project advancing to tender award / construction.

4.5 ENVIRONMENTAL ASSESSMENT

An EIA screening exercise was undertaken to determine if EIA is required for the proposed development as set out in the mandatory and discretionary provisions of the Planning and Development Act 2000, as amended (the Act) and as set out in Schedule 5 of the Planning and Development Regulations, 2001 as amended (the Regulations). The proposed development

consists of road construction works which do not exceed the threshold for Environmental Impact Assessment set out in Schedule 5 of the Regulations. In accordance with Article 120(1)(b)(i) of those regulations, based on environmental assessments carried out and considering the nature, size and location of the development it is concluded that an EIA for the proposed development is not required as there is no potential for significant effects, including cumulative effects, on the environment. The EIA screening report is contained in Appendix C.

An Appropriate Assessment Screening Report was undertaken (on behalf of the respective Authorities) in accordance with the requirements of Article 6(3) and Article 6(4) of the EU Habitats Directive (92/43/EEC). The constraints study area along with a preliminary design was provided to an Environmental Consultant to inform the process. The Screening Report concluded the proposed development on its own, or in cumulation with other projects, will not have a significant effect on European sites. The AA Screening Report is contained in Appendix D.

4.6 CONSTRUCTION

The construction of the project is likely to take approximately 6 months. Traffic management will be required.

4.7 CONCLUSION

The information contained in this report together with the drawings provided describe the nature, extents and principal features of the proposed works as required under Part 8 of the Planning & Development Regulations 2001 (as amended).

The background of the proposed project and its associated aims and objectives have been identified in section 1.0 of this report. It has been outlined in Section 2.0 that the proposed scheme is consistent with European, National, Regional and Local Policy objectives. The potential environmental impacts arising from the works have been considered and it is concluded that construction works associated with the scheme will have no significant impact on the receiving environment if constructed in accordance with the proposed design and good practice.

It is therefore concluded that the proposed development of the Active Travel Scheme in Bundoran/Ballyshannon is in line with proper planning and sustainable development of the area in which the project is proposed.

Submissions or observations with respect to the proposed development, dealing with the proper planning and sustainable development of the area in which the development would be situated, may be made in writing to The County Secretariat Office, Donegal County Council, County House, Lifford, Co. Donegal F93 Y622, or emailed to activetravel@donegalcoco.ie before 4:00pm on Thursday 21st September, 2023 as outlined in the public notices.

Appendix A – Drawings

Preliminary Drawings for Bundoran Ballyshannon Active Travel Project

Date: July 2023

Issue: Final

Revision: C

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Údarás Náisiúnta Iompair
National Transport Authority



An Roinn Iompair
Department of Transport

Preliminary Drawings
for
Bundoran Ballyshannon Active Travel Scheme
July 2023

Schedule of Drawings

<u>DRAWING NO.</u>	<u>TITLE</u>	<u>REVISION</u>
11199-08-1000	Site Location	B
11199-08-1001	Site Extents Sheet 01 of 08	C
11199-08-1002	Site Extents Sheet 02 of 08	C
11199-08-1003	Site Extents Sheet 03 of 08	C
11199-08-1004	Site Extents Sheet 04 of 08	C
11199-08-1005	Site Extents Sheet 05 of 08	C
11199-08-1006	Site Extents Sheet 06 of 08	C
11199-08-1007	Site Extents Sheet 07 of 08	C
11199-08-1008	Site Extents Sheet 08 of 08	C

<u>DRAWING NO.</u>	<u>TITLE</u>	<u>REVISION</u>
11199-08-1010	General Arrangement Sheet 01 of 08	C
11199-08-1011	General Arrangement Sheet 01 of 08	C
11199-08-1012	General Arrangement Sheet 01 of 08	C
11199-08-1013	General Arrangement Sheet 01 of 08	C
11199-08-1014	General Arrangement Sheet 01 of 08	C
11199-08-1015	General Arrangement Sheet 01 of 08	C
11199-08-1016	General Arrangement Sheet 01 of 08	C
11199-08-1017	General Arrangement Sheet 01 of 08	C
11199-08-1020	Typical Cross Section Sheet 01 of 02	C
11199-08-1021	Typical Cross Section Sheet 0 of 02	C



IRELAND SITE LOCATION MAP

Scale at A1 1:2,000,000
Scale at A3 1:4,000,000



Rev	Date	Description	By	Chkd.
B	20.06.23	Draft	AA	RF

Client:



Donegal County Council

Project:

Bundoran - Ballyshannon

Title:

Site Location Map

Prepared by:

CQ

Checked:

RF

Date:

20.06.23

Project Director:

John O'Flaherty

Drawing Status:

Planning

Scale

@ A1,1:1000



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www.tobin.ie

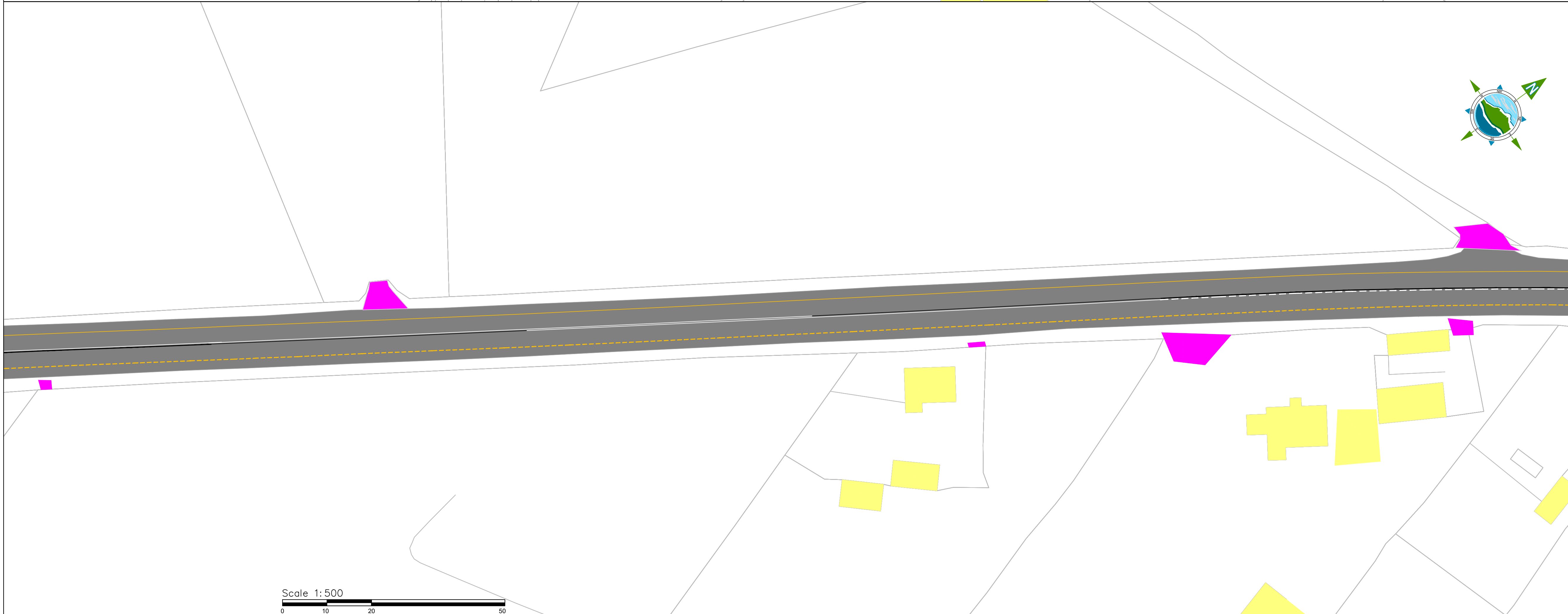
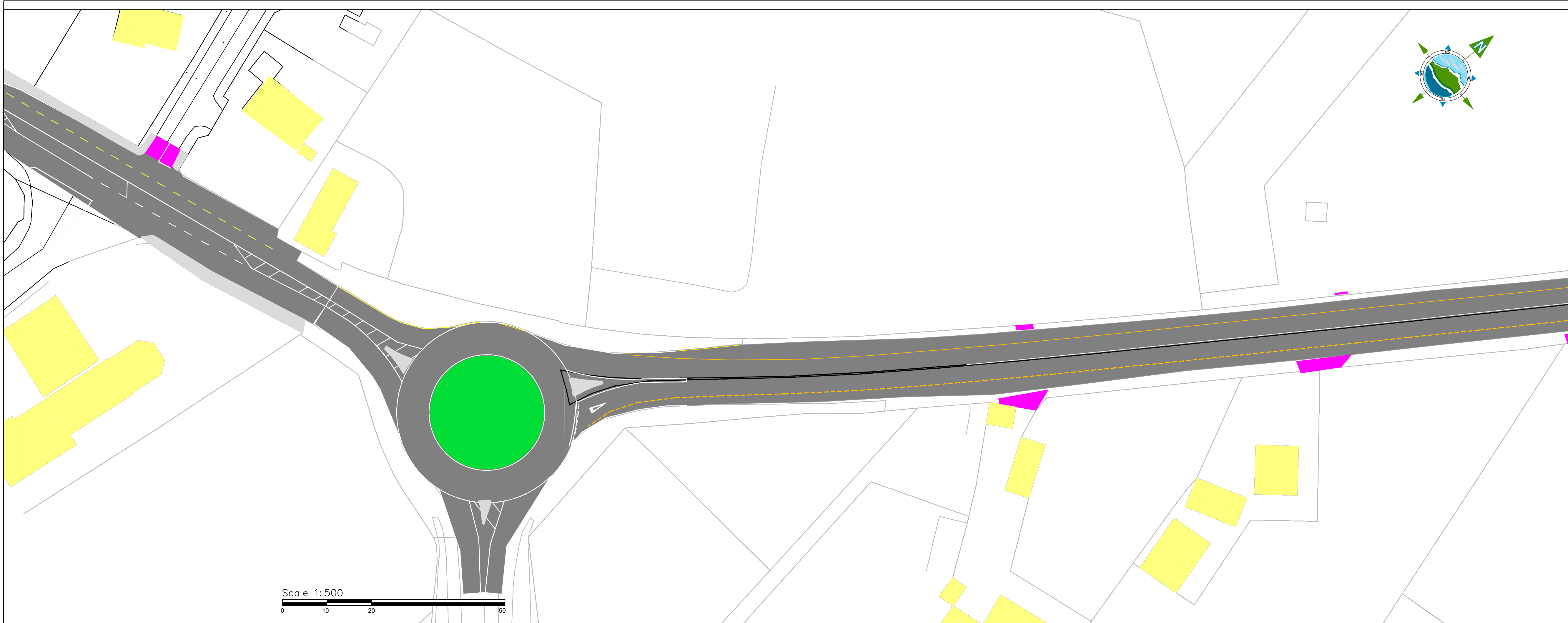
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Drawing No.:

11199-08-1000

Revision:

B



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ITM COORDINATE SYSTEM

LEGEND

- EXISTING CARRIAGEWAY
- EXISTING ACCESS
- EXISTING FOOTPATH
- EXISTING LOCATION LAMP POST

Rev	Date	Description	By	Chk.	Appr.
B	12.06.23				
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

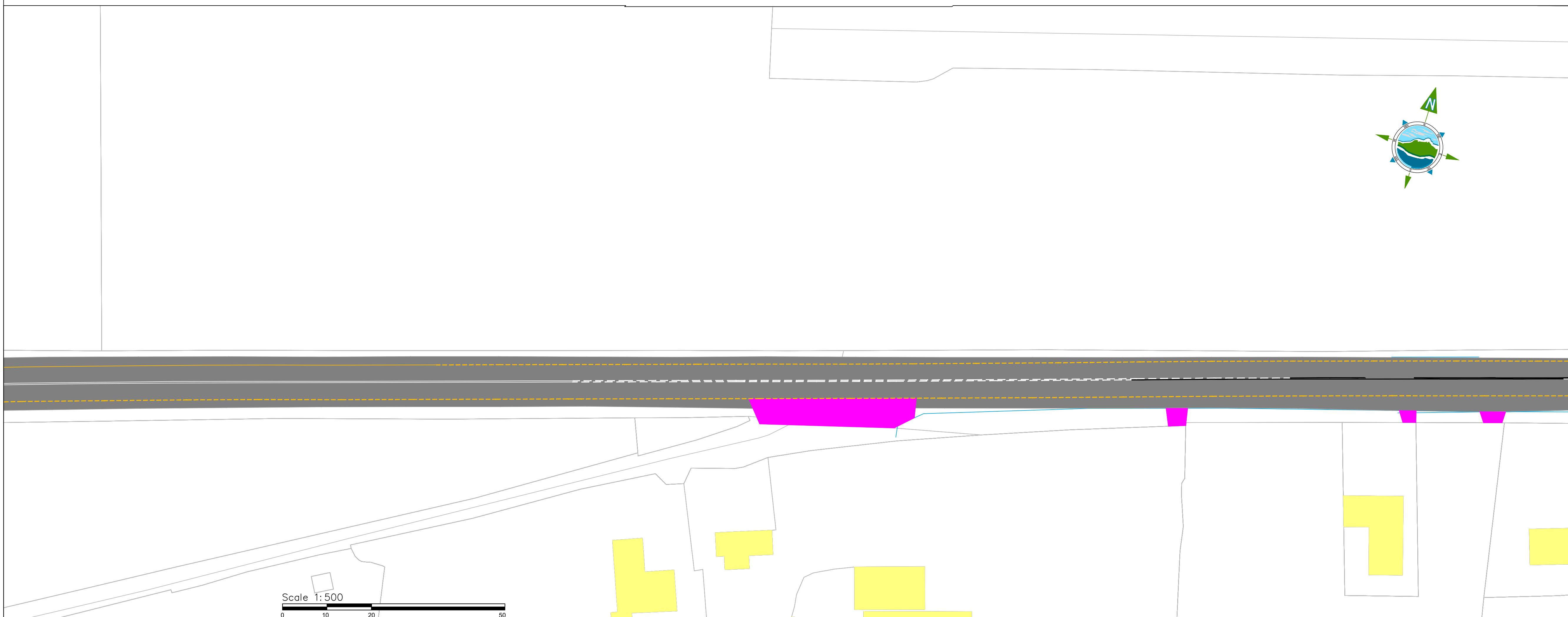
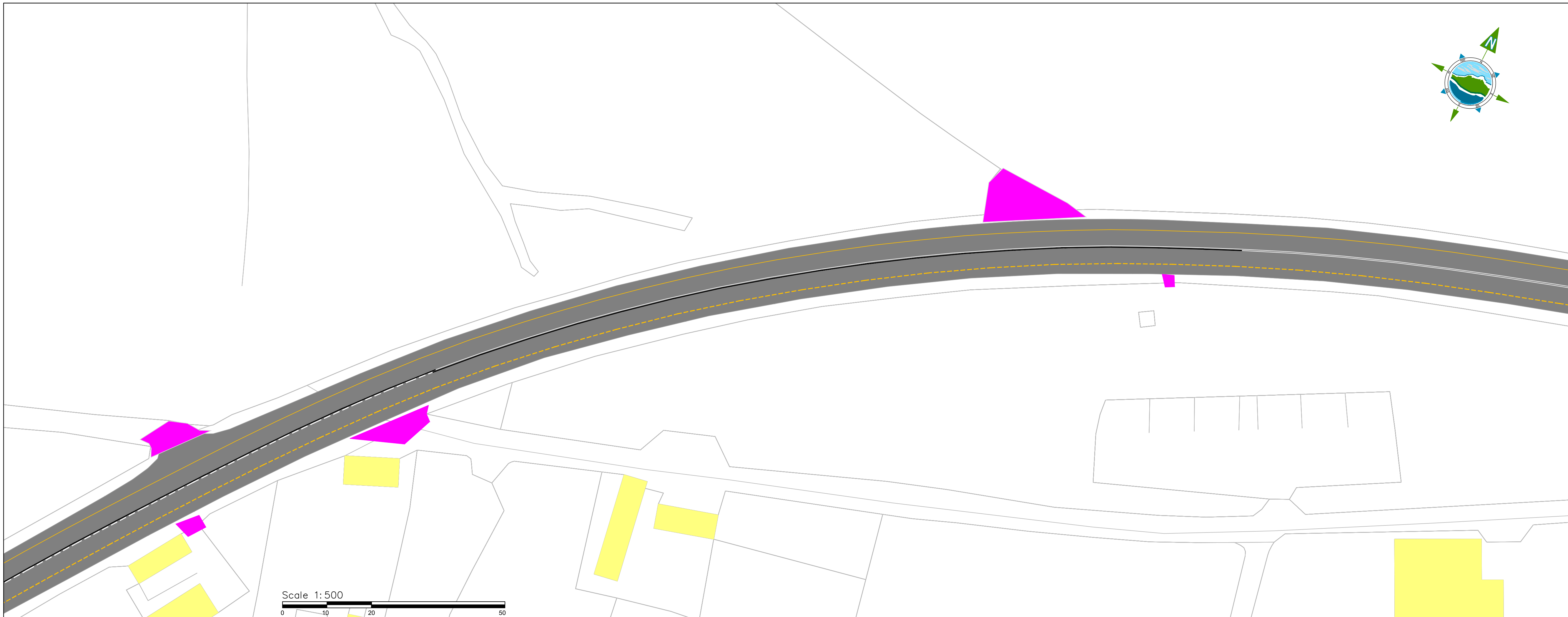
Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
Existing Layout
Sheet 01 of 08

A1 Scale: 1:500 A3 Scale: 1:1000
Prepared by: CQ Checked: RF Date: 12.06.23
Project Director: J. O'Flaherty
Drawing Status: Planning


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Drawing No.: 11199-08-1001 Revision: B



THE INFORMATION ON THIS DRAWING
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LEGEND

- EXISTING CARRIAGEWAY
- EXISTING ACCESS
- EXISTING FOOTPATH
- EXISTING LOCATION LAMP POST

Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
Existing Layout
Sheet 02 of 08



A1 Scale: 1:500 A3 Scale: 1:1000
Prepared by: CQ Checked: RF Date: 12.06.23
Project Director: J. O'Flaherty
Drawing Status: Planning


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Drawing No.: 11199-08-1002 Revision: B

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ITM COORDINATE SYSTEM

LEGEND

- EXISTING CARRIAGEWAY 
- EXISTING ACCESS 
- EXISTING FOOTPATH 
- EXISTING LOCATION LAMP POST 



Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

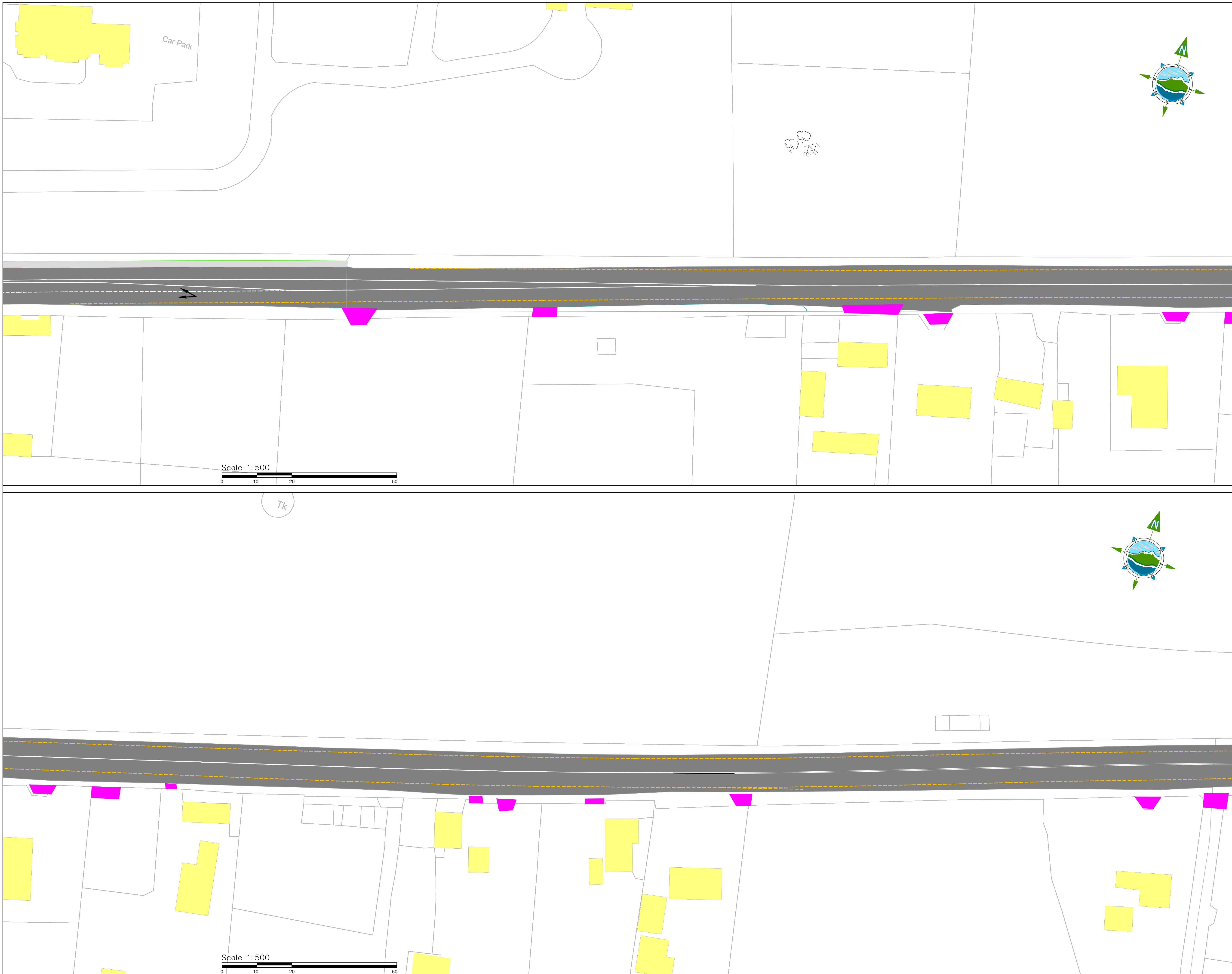
Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
Existing Layout
Sheet 03 of 08

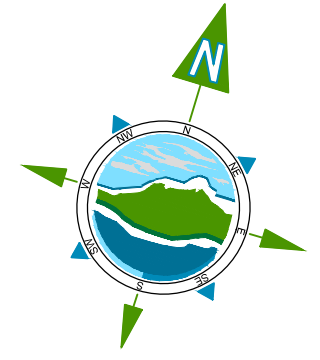
A1 Scale: 1:500 A3 Scale: 1:1000
Prepared by: CQ Checked: RF Date: 06.03.23
Project Director: J. O'Flaherty
Drawing Status: Planning


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Drawing No.: 11199-08-1003 Revision: B



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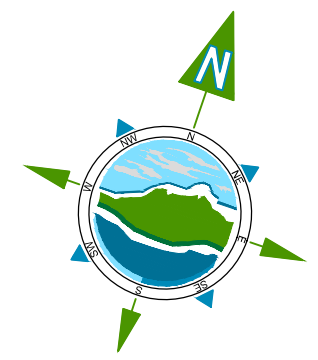


LEGEND

- EXISTING CARRIAGEWAY
- EXISTING ACCESS
- EXISTING FOOTPATH
- EXISTING LOCATION LAMP POST

Scale 1:500
0 10 20 50

Tk



Scale 1:500
0 10 20 50

Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client:
Donegal County Council

Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
Existing Layout
Sheet 04 of 08

A1 Scale: 1:500 A3 Scale: 1:1000
Prepared by: CQ Checked: RF Date: 12.06.23
Project Director: J. O'Flaherty
Drawing Status: Planning

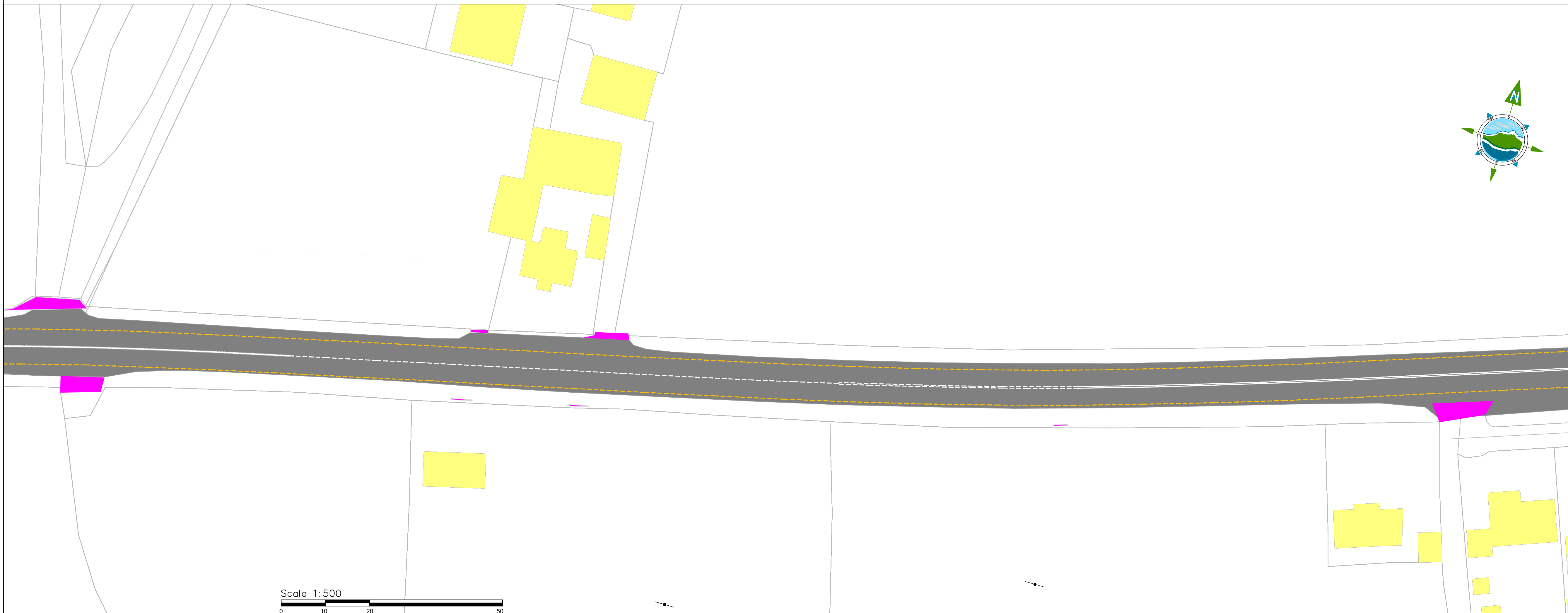
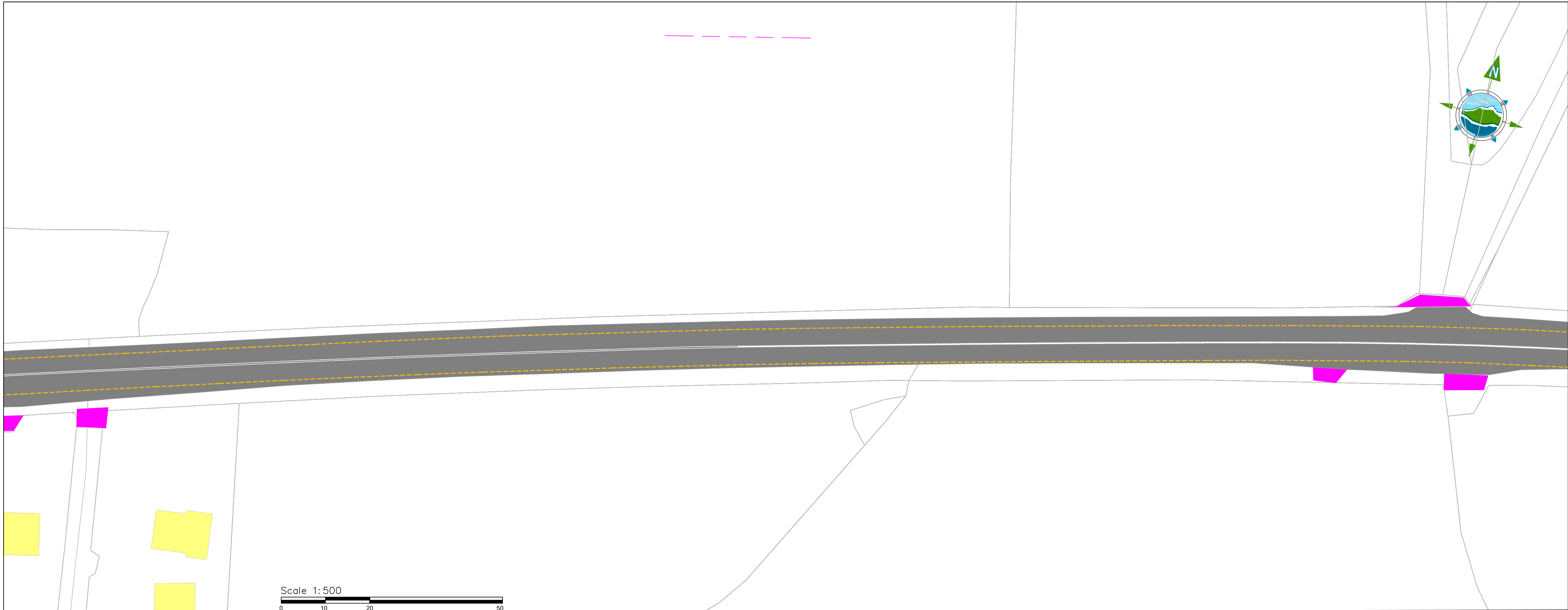
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Drawing No.: 11199-08-1004 Revision: B

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LEGEND

- EXISTING CARRIAGEWAY 
- EXISTING ACCESS 
- EXISTING FOOTPATH 
- EXISTING LOCATION LAMP POST 



Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

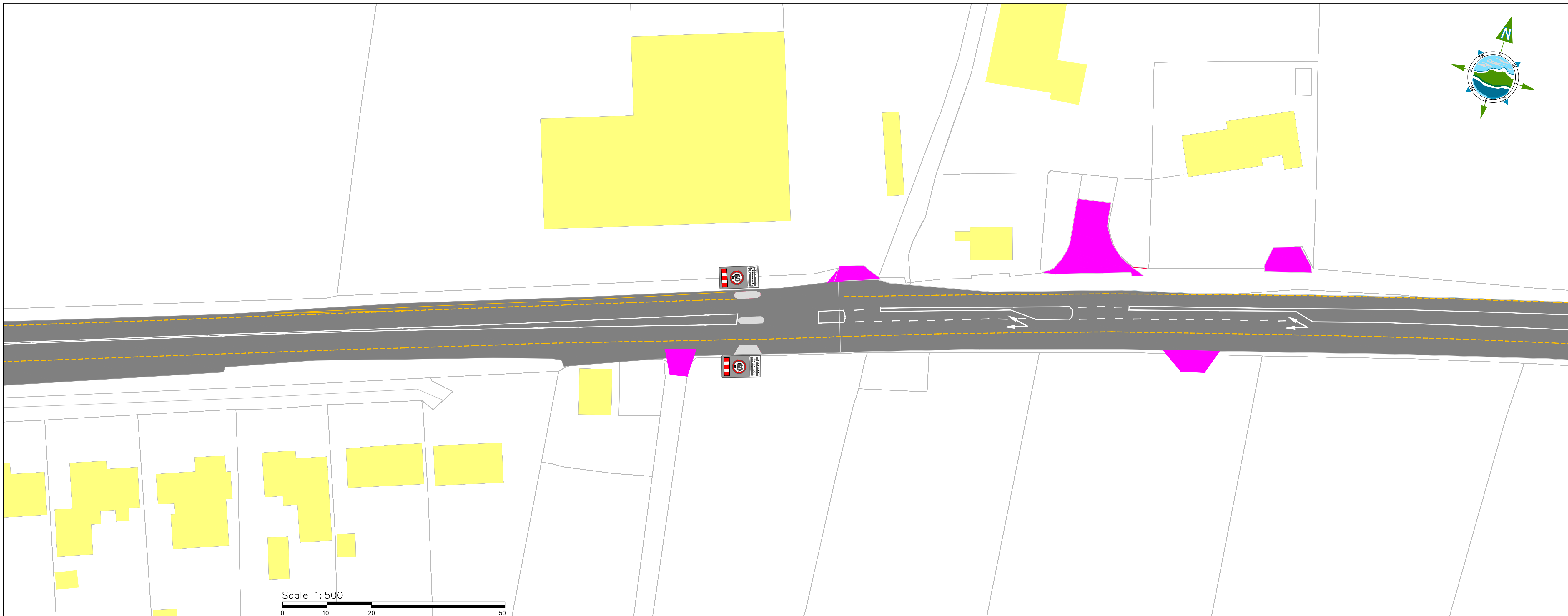
Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
Existing Layout
Sheet 05 of 08

A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Project Director: J. O'Flaherty	Date: 12.06.23
Drawing Status: Planning	


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Drawing No.: 11199-08-1005	Revision: B
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LEGEND

- EXISTING CARRIAGEWAY
- EXISTING ACCESS
- EXISTING FOOTPATH
- EXISTING LOCATION LAMP POST

Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SD			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon Existing Layout Sheet 06 of 08

A1 Scale: 1:500 A3 Scale: 1:1000
 Prepared by: CQ Checked: RF Date: 12.06.23
 Project Director: J. O'Flaherty
 Drawing Status: Planning

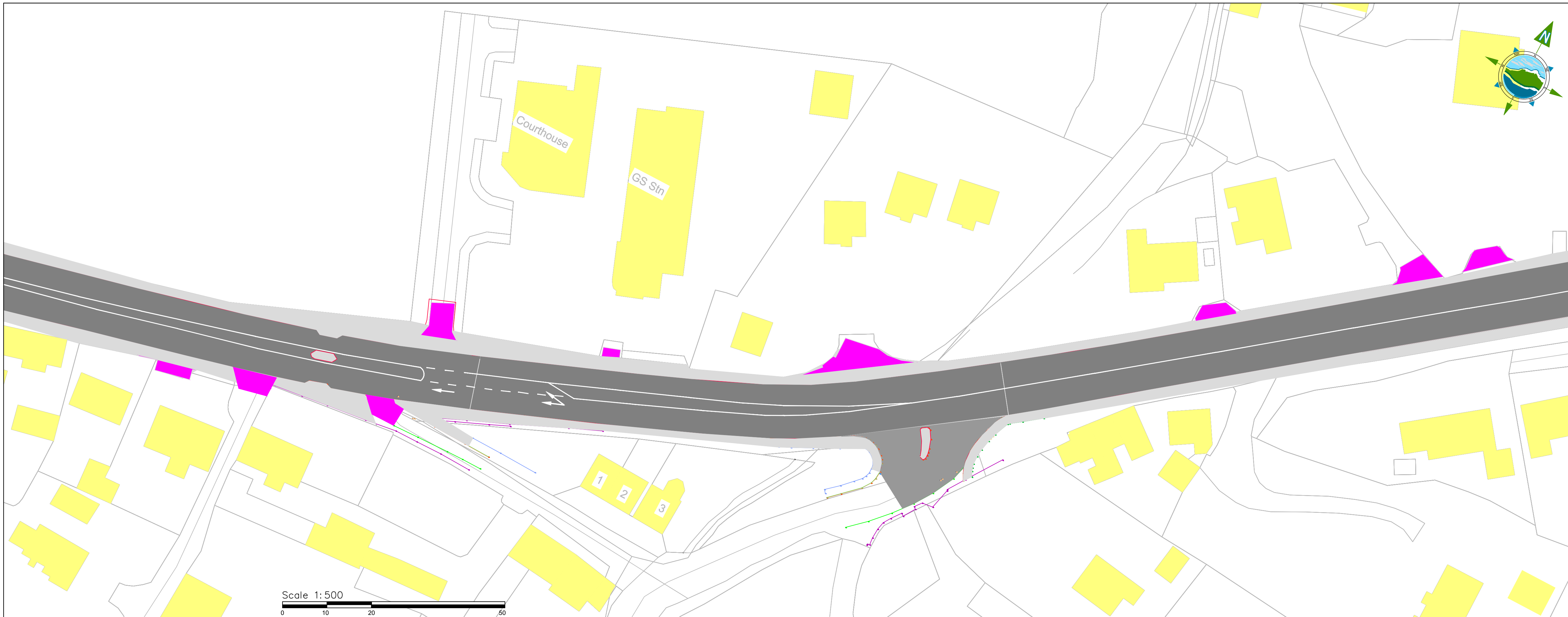
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Drawing No.: 11199-08-1006 Revision: B

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LEGEND

- EXISTING CARRIAGEWAY 
- EXISTING ACCESS 
- EXISTING FOOTPATH 
- EXISTING LOCATION LAMP POST 



Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon Existing Layout Sheet 07 of 08

A1 Scale: 1:500 A3 Scale: 1:1000
 Prepared by: CQ Checked: RF Date: 12.06.23
 Project Director: J. O'Flaherty
 Drawing Status: Planning

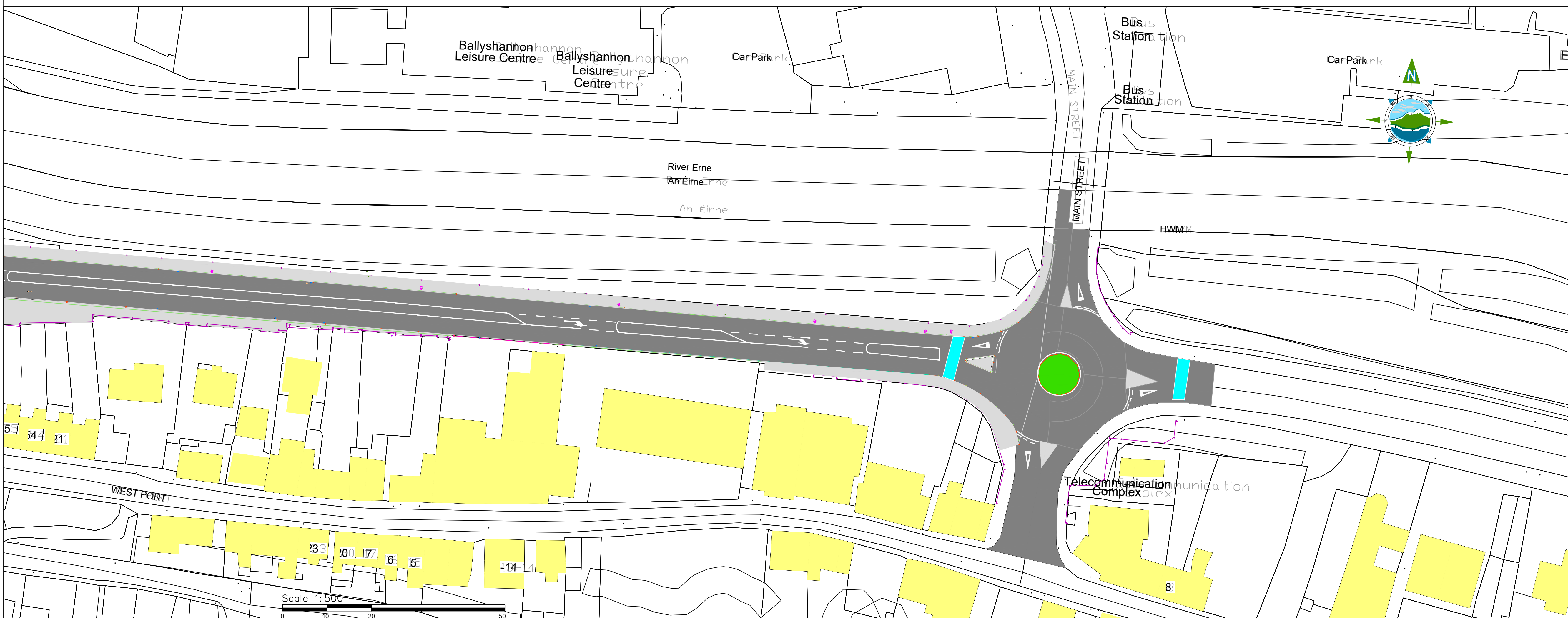
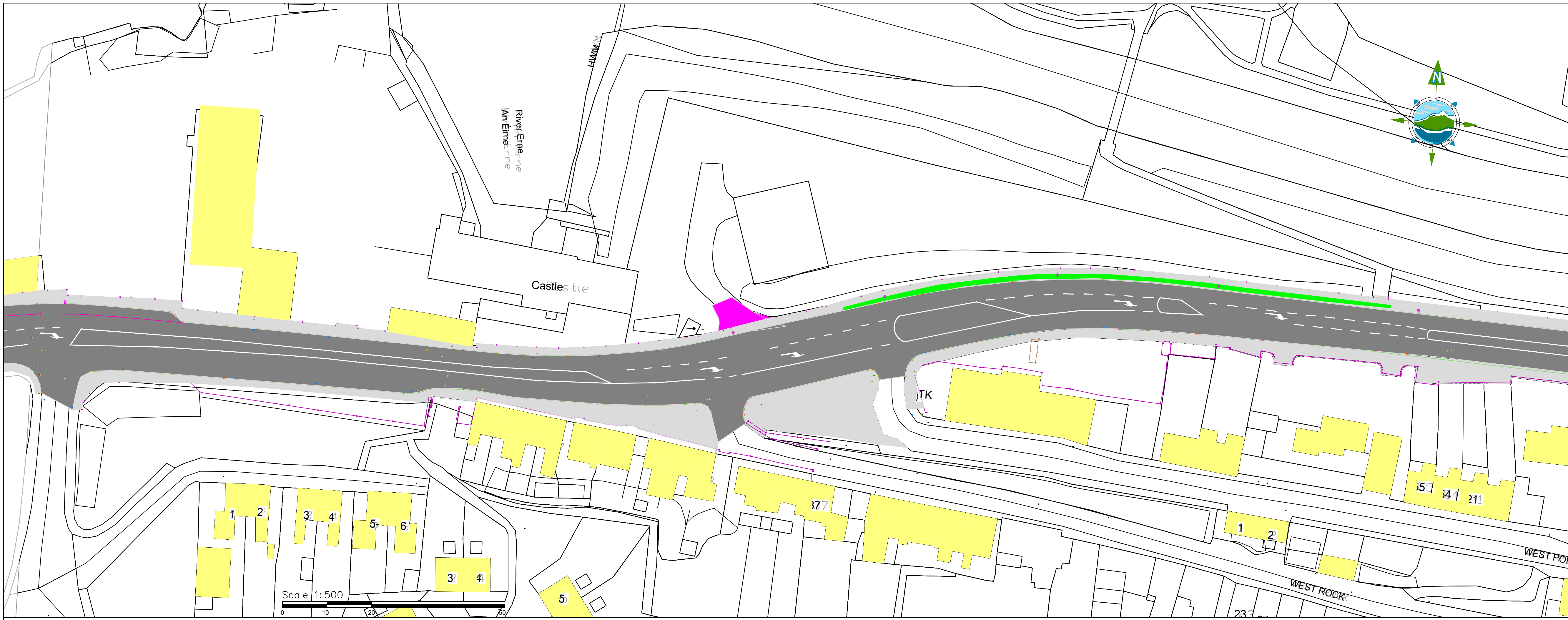

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Drawing No.: 11199-08-1007 Revision: B

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LEGEND

- EXISTING CARRIAGEWAY 
- EXISTING ACCESS 
- EXISTING FOOTPATH 
- EXISTING LOCATION LAMP POST 



Rev	Date	Description	By	Chk.	Appr.
B	12.06.23	Draft			
A	06.03.23	Draft SB			

File Name: DCCAT-TOBIN-DR-GA-Exinting

Client: 
Donegal County Council

Project:
11199 Donegal Co Co Active Travel

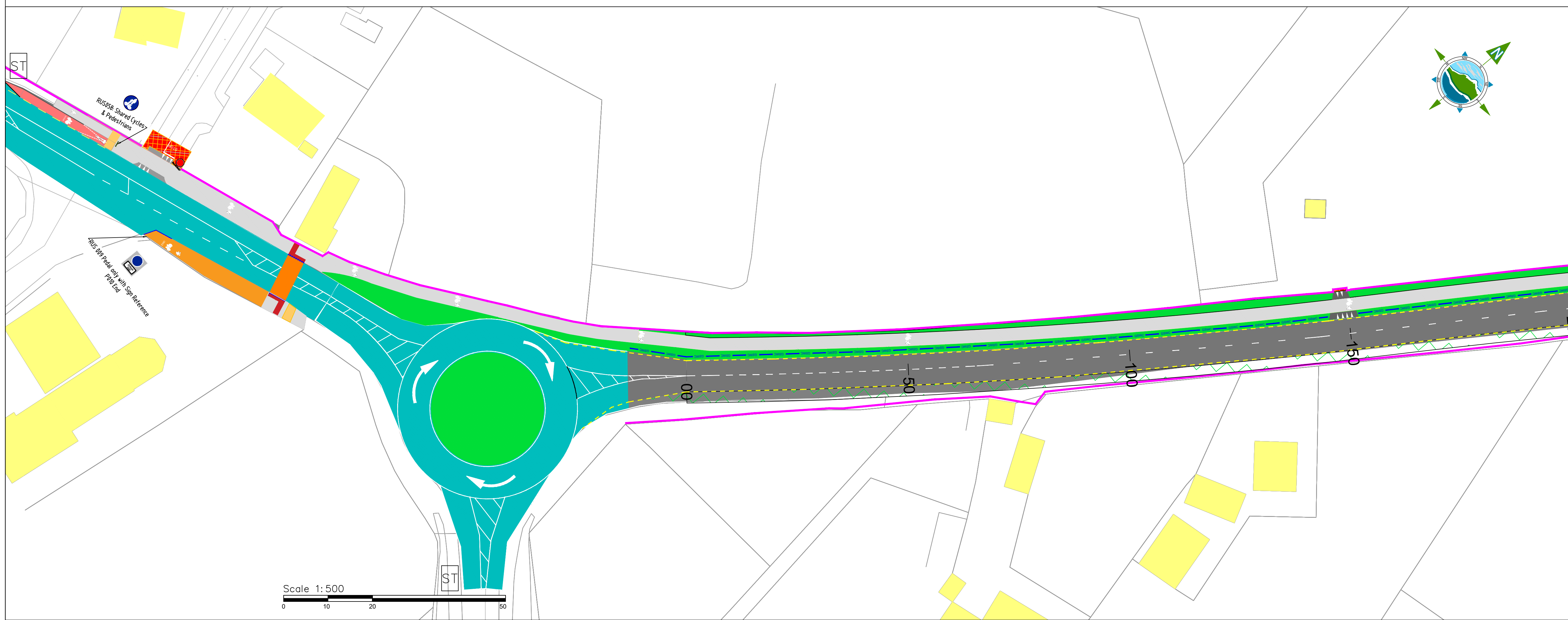
Title:
Bundoran to Ballyshannon
General Arrangement
Sheet 08 of 08

A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Date: 12.06.23	
Project Director: J. O'Flaherty	
Drawing Status: Planning	


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Drawing No.: 11199-08-1008	Revision: B
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ITM COORDINATE SYSTEM



LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH ■
- PROPOSED CARRIAGEWAY REALIGNMENT ■
- PROPOSED SHARED SURFACE ■
- PROPOSED CYCLELANE ■
- PROPOSED VERGE ■
- EXISTING CARRIAGEWAY ■
- EXISTING ACCESS ■
- EXISTING FOOTPATH ■
- PROPOSED CYCLE FRIENDLY GULLY ■
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕

Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	LOK
B	12.06.23	Draft	CP	RF	LOK
A	06.03.23	Draft SD	CP	RF	LOK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

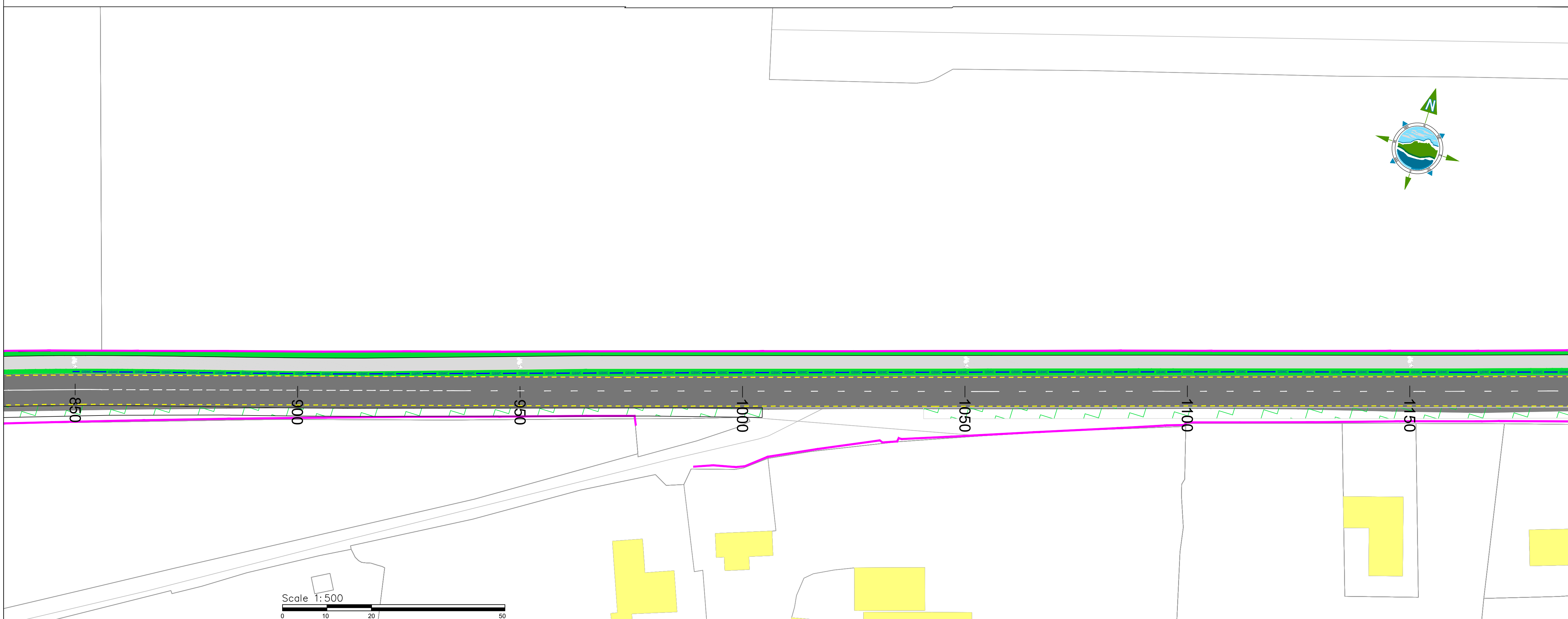
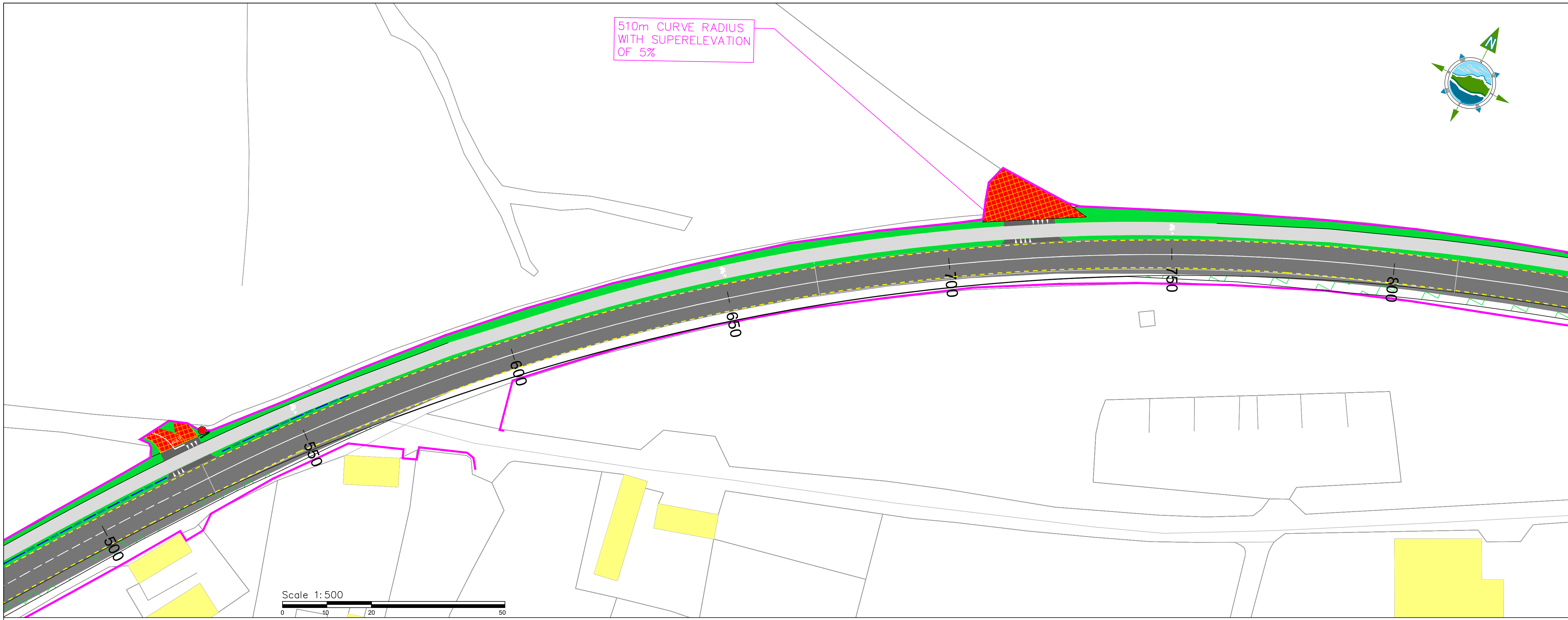
Title: Bundoran to Ballyshannon General Arrangement Sheet 01 of 08

A1 Scale: 1:500		A3 Scale: 1:1000
Prepared by: CQ	Checked: RF	Date: 21.07.23
Project Director: J. O'Flaherty		
Drawing Status: Planning		

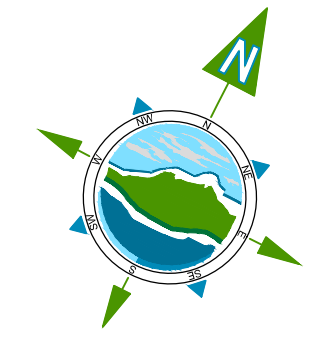
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Drawing No.: 11199-08-1010	Revision: C
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LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH ■
- PROPOSED CARRIAGEWAY REALIGNMENT ■
- PROPOSED SHARED SURFACE ■
- PROPOSED CYCLELANE ■
- PROPOSED VERGE ■
- EXISTING CARRIAGEWAY ■
- EXISTING ACCESS ■
- EXISTING FOOTPATH ■
- PROPOSED CYCLE FRIENDLY GULLY ■
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕

Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	LOK
B	12.06.23	Draft	CP	RF	LOK
F	06.03.23	Draft SD	CP	RF	LOK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon General Arrangement Sheet 02 of 08

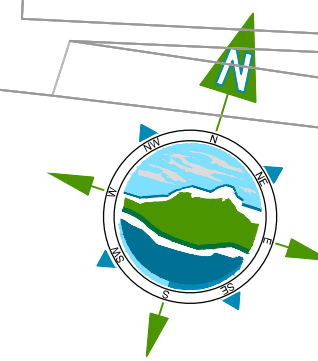
A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Project Director: J. O'Flaherty	Date: 21.07.23
Drawing Status: Planning	

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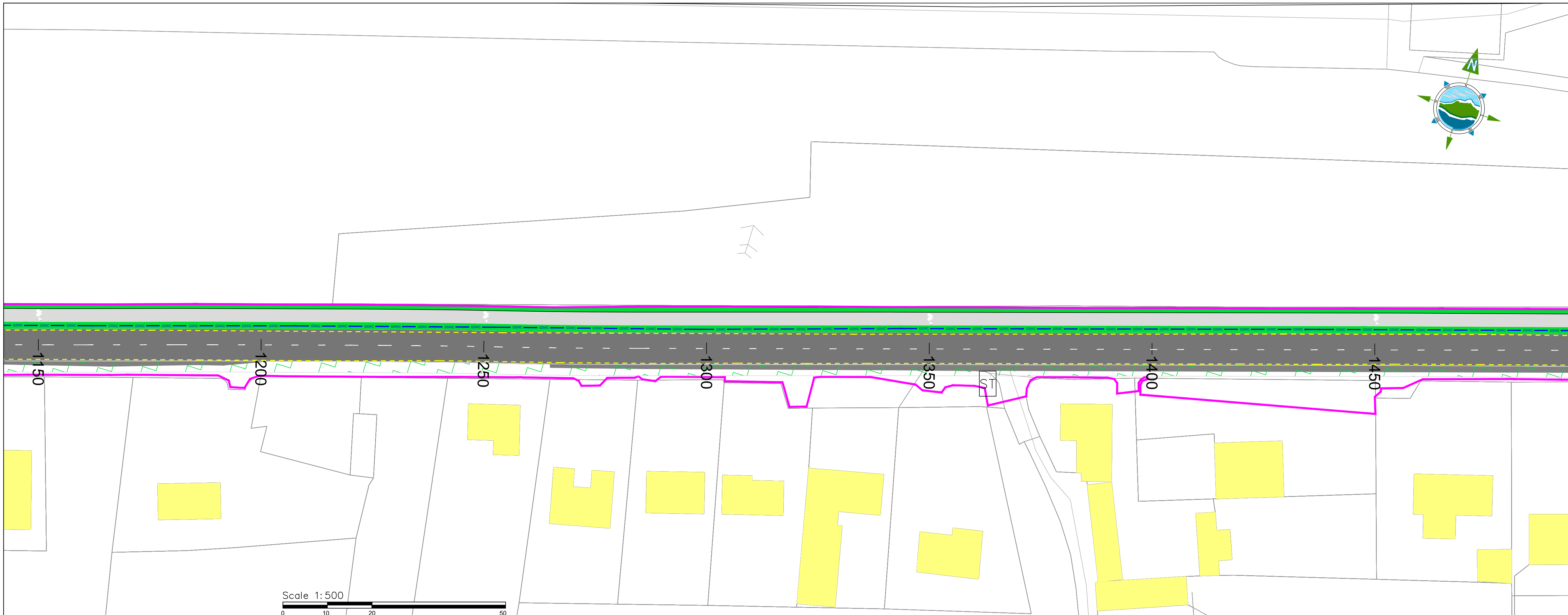
Drawing No.: 11199-08-1011	Revision: C
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ITM COORDINATE SYSTEM




LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH —
- PROPOSED CARRIAGEWAY REALIGNMENT —
- PROPOSED SHARED SURFACE —
- PROPOSED CYCLELANE —
- PROPOSED VERGE —
- EXISTING CARRIAGEWAY —
- EXISTING ACCESS —
- EXISTING FOOTPATH —
- PROPOSED CYCLE FRIENDLY GULLY —
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕



Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	JK
B	12.06.23	Draft	CP	RF	JK
A	06.03.23	Draft SD	CP	RF	JK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client:  Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon General Arrangement Sheet 03 of 08

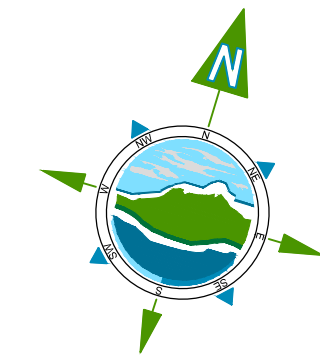
A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Project Director: J. O'Flaherty	Date: 21.07.23
Drawing Status: Planning	

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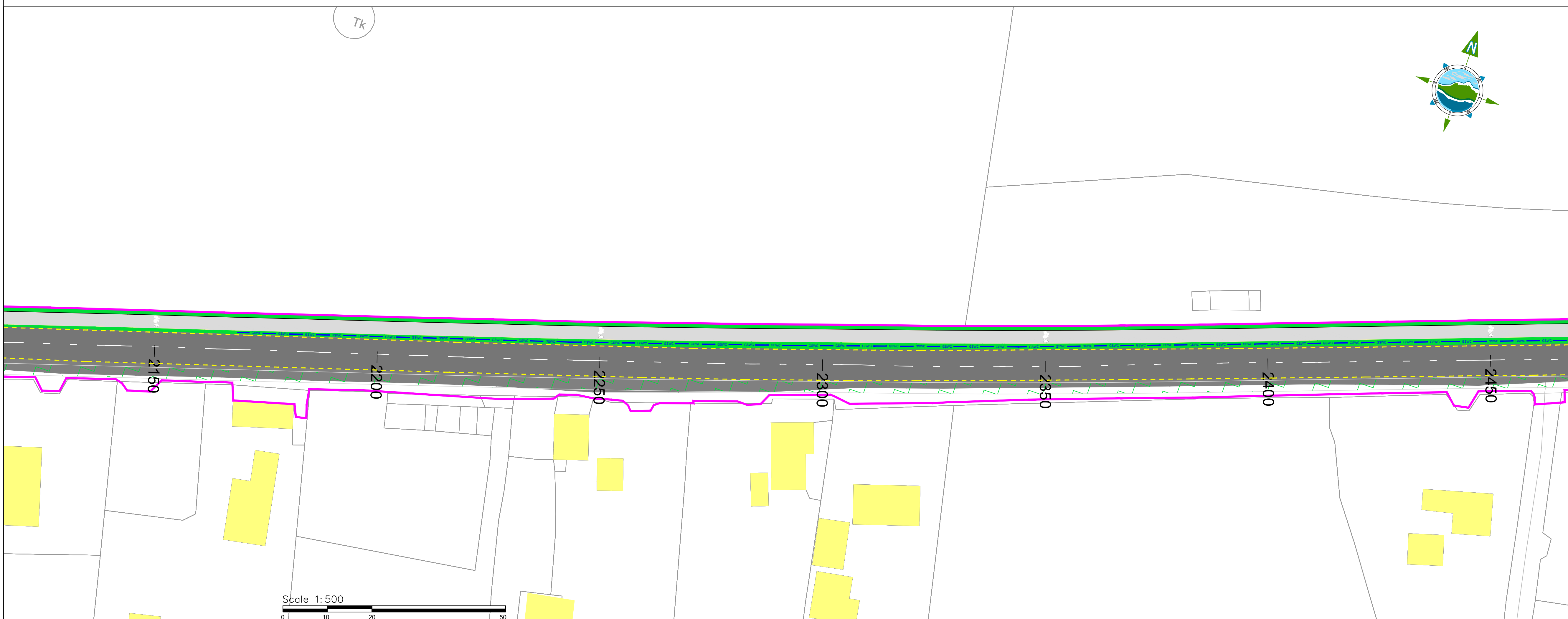
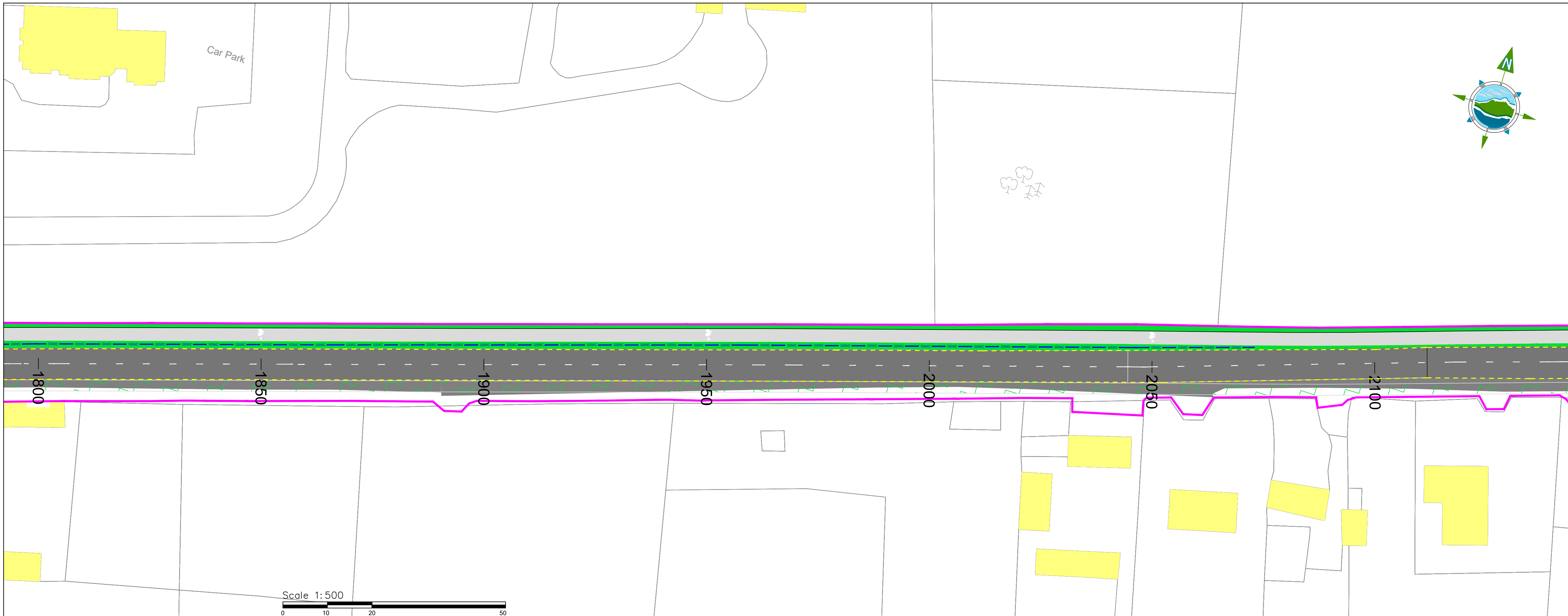
Drawing No.: 11199-08-1012	Revision: C
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THE INFORMATION ON THIS DRAWING
IS TO THE ORDNANCE SURVEY IRELAND
ITM COORDINATE SYSTEM



LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH ■
- PROPOSED CARRIAGEWAY REALIGNMENT ■
- PROPOSED SHARED SURFACE ■
- PROPOSED CYCLELANE ■
- PROPOSED VERGE ■
- EXISTING CARRIAGEWAY ■
- EXISTING ACCESS ■
- EXISTING FOOTPATH ■
- PROPOSED CYCLE FRIENDLY GULLY ■
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕



C	21.07.23	Planning	CP	RF	JKK
B	12.06.23	Draft	CP	RF	JKK
A	06.03.23	Draft SD	CP	RF	JKK
Rev	Date	Description	By	Chk.	Appr.

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon General Arrangement Sheet 04 of 08

A1 Scale: 1:500 A3 Scale: 1:1000
 Prepared by: CQ Checked: RF Date: 21.07.23
 Project Director: J. O'Flaherty
 Drawing Status: Planning

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Drawing No.: 11199-08-1013 Revision: C

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ITM COORDINATE SYSTEM

LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH ■
- PROPOSED CARRIAGEWAY REALIGNMENT ■
- PROPOSED SHARED SURFACE ■
- PROPOSED CYCLELANE ■
- PROPOSED VERGE ■
- EXISTING CARRIAGEWAY ■
- EXISTING ACCESS ■
- EXISTING FOOTPATH ■
- PROPOSED CYCLE FRIENDLY GULLY ■
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕

Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	JK
B	12.06.23	Draft	CP	RF	JK
A	06.03.23	Draft SD	CP	RF	JK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: 
Donegal County Council

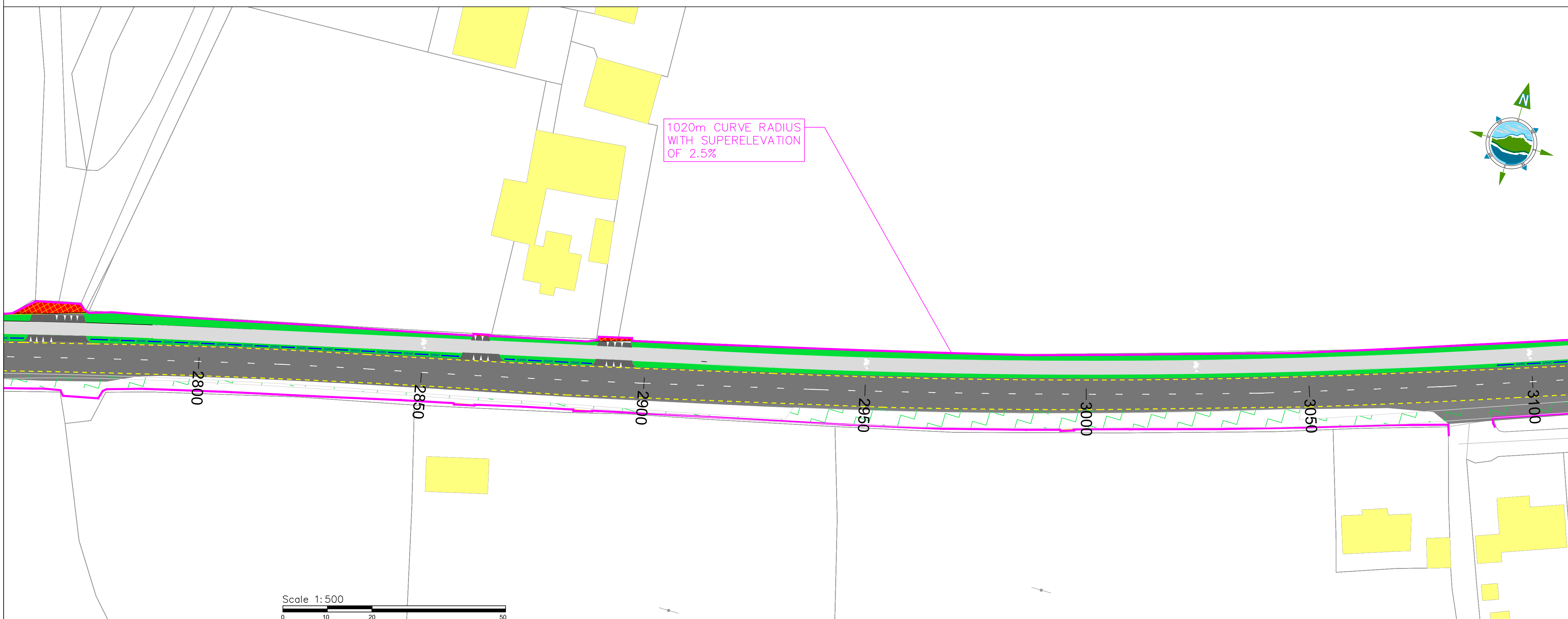
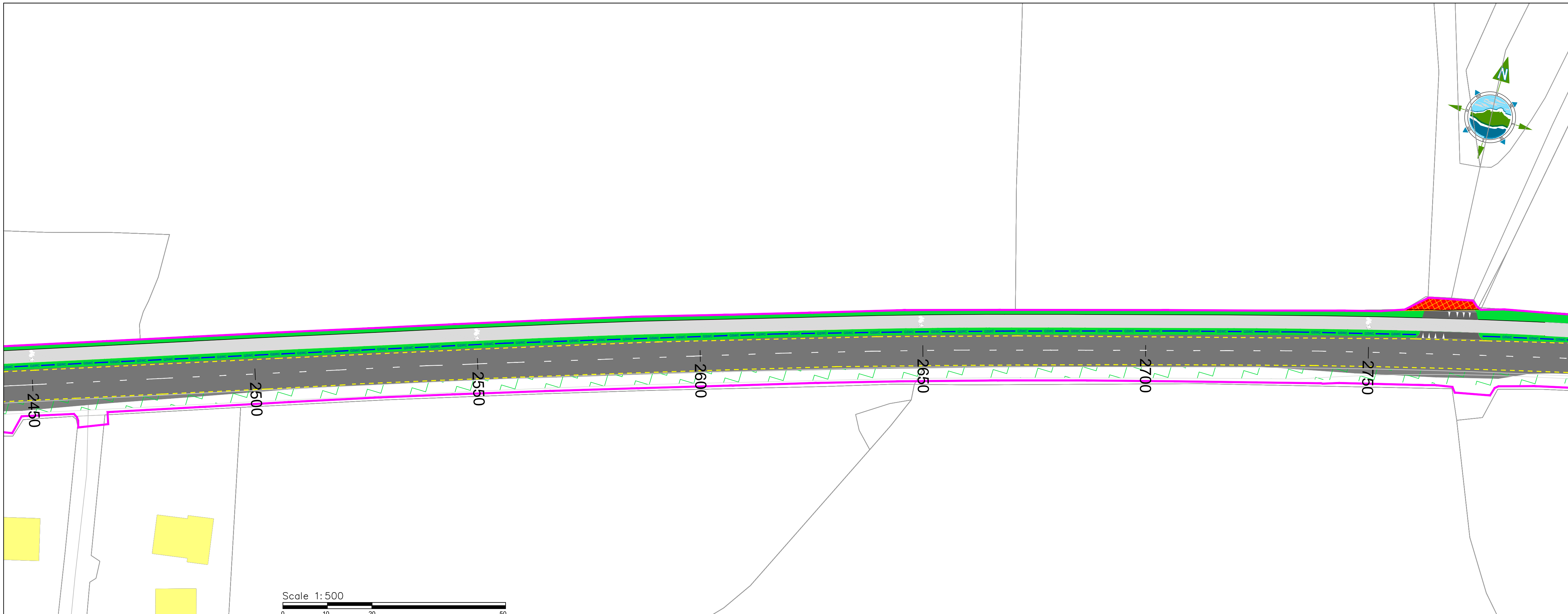
Project:
11199 Donegal Co Co Active Travel

Title:
Bundoran to Ballyshannon
General Arrangement
Sheet 05 of 08

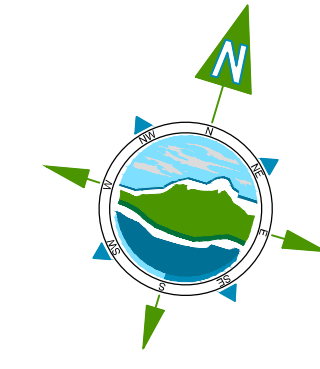
A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Date: 21.07.23	
Project Director: J. O'Flaherty	
Drawing Status: Planning	


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Drawing No.: 11199-08-1014 Revision: C

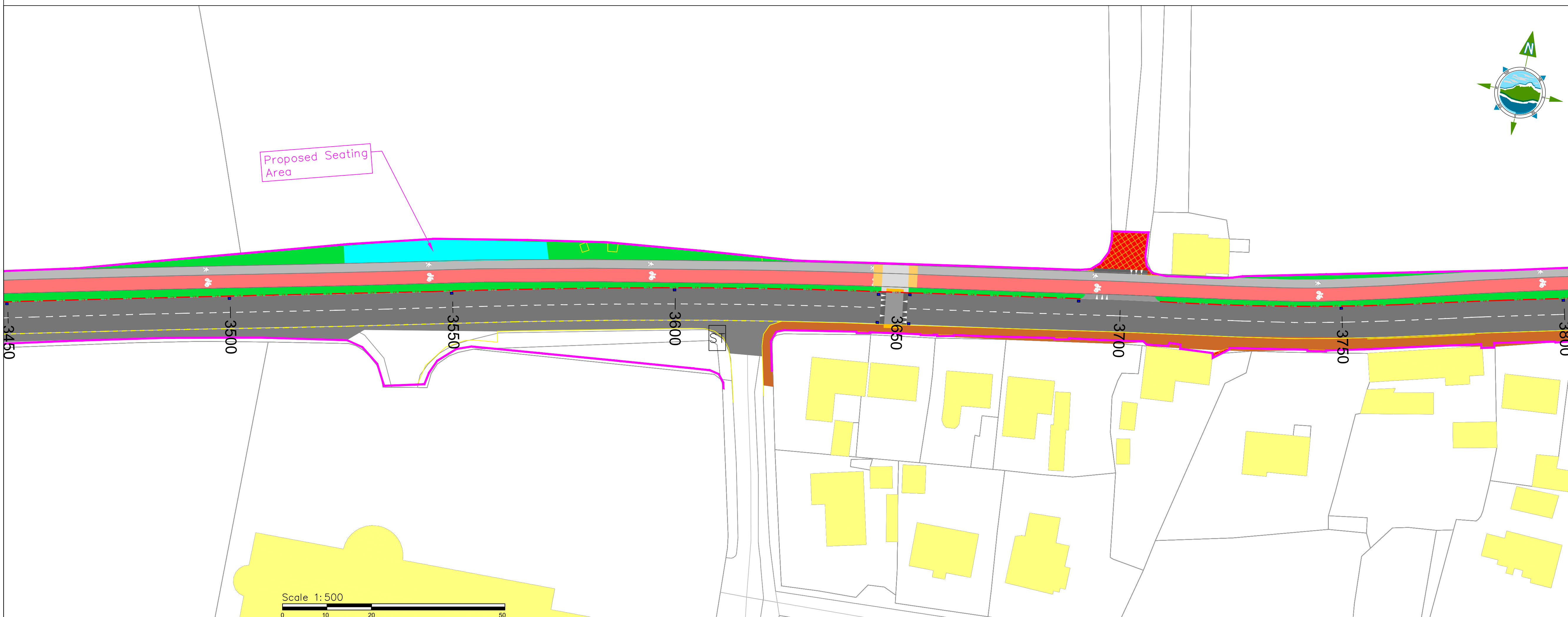
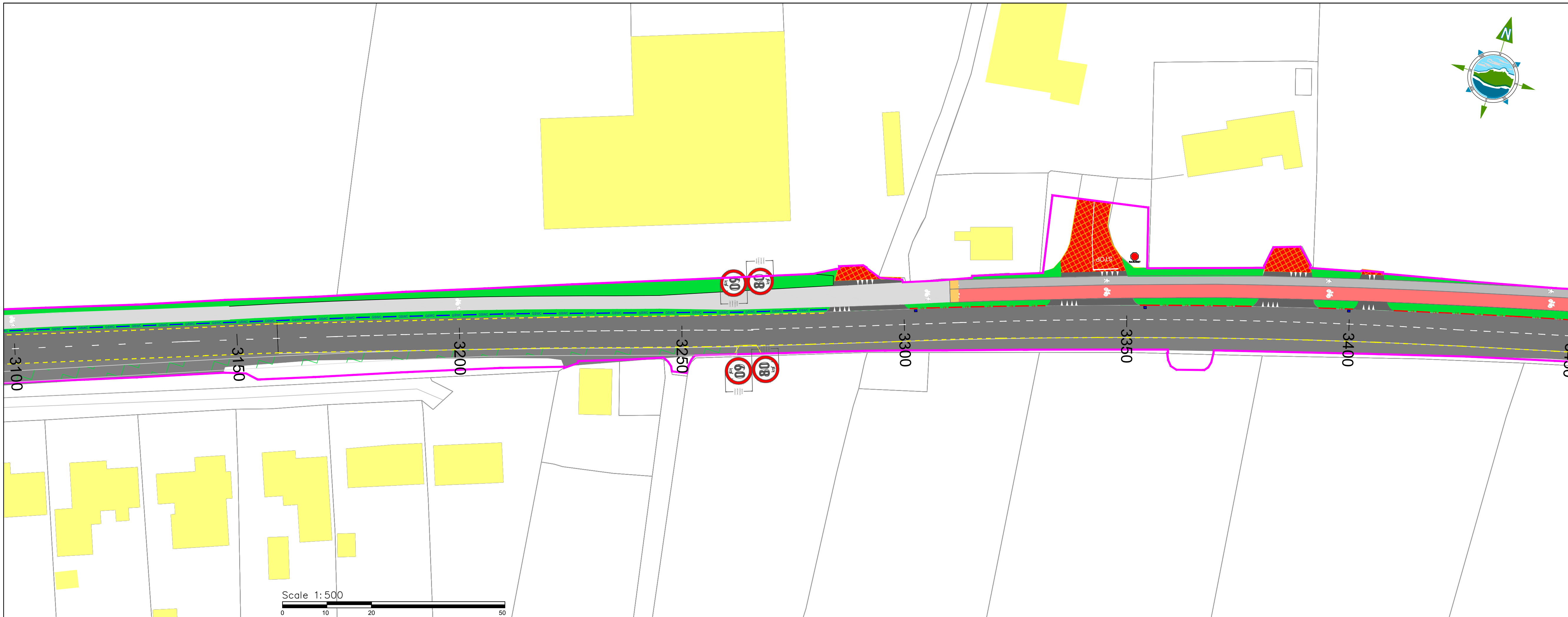


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LEGEND

- FIXED BOUNDARY —
- PROPOSED FOOTPATH —
- PROPOSED CARRIAGEWAY REALIGNMENT —
- PROPOSED SHARED SURFACE —
- PROPOSED CYCLELANE —
- PROPOSED VERGE —
- EXISTING CARRIAGEWAY —
- EXISTING ACCESS —
- EXISTING FOOTPATH —
- PROPOSED CYCLE FRIENDLY GULLY —
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕



Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	JK
B	12.06.23	Draft	CP	RF	JK
A	06.03.23	Draft SD	CP	RF	JK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon General Arrangement Sheet 06 of 08

A1 Scale: 1:500	A3 Scale: 1:1000
Prepared by: CQ	Checked: RF
Project Director: J. O'Flaherty	Date: 21.07.23
Drawing Status: Planning	

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Drawing No.: 11199-08-1015	Revision: C
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LEGEND

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- PROPOSED FOOTPATH ■
- PROPOSED CARRIAGEWAY REALIGNMENT ■
- PROPOSED SHARED SURFACE ■
- PROPOSED CYCLELANE ■
- PROPOSED VERGE ■
- EXISTING CARRIAGEWAY ■
- EXISTING ACCESS ■
- EXISTING FOOTPATH ■
- PROPOSED CYCLE FRIENDLY GULLY ■
- PROPOSED SLIPFORM KERB —
- PROPOSED GRASS SURFACE WATER CHANNEL —
- EXISTING LOCATION LAMP POST ⊕
- PROPOSED LOCATION LAMP POST ⊕

Rev	Date	Description	By	Chk.	Appr.
C	21.07.23	Planning	CP	RF	JK
B	12.06.23	Draft	CP	RF	JK
A	06.03.23	Draft SD	CP	RF	JK


File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: 
Donegal County Council

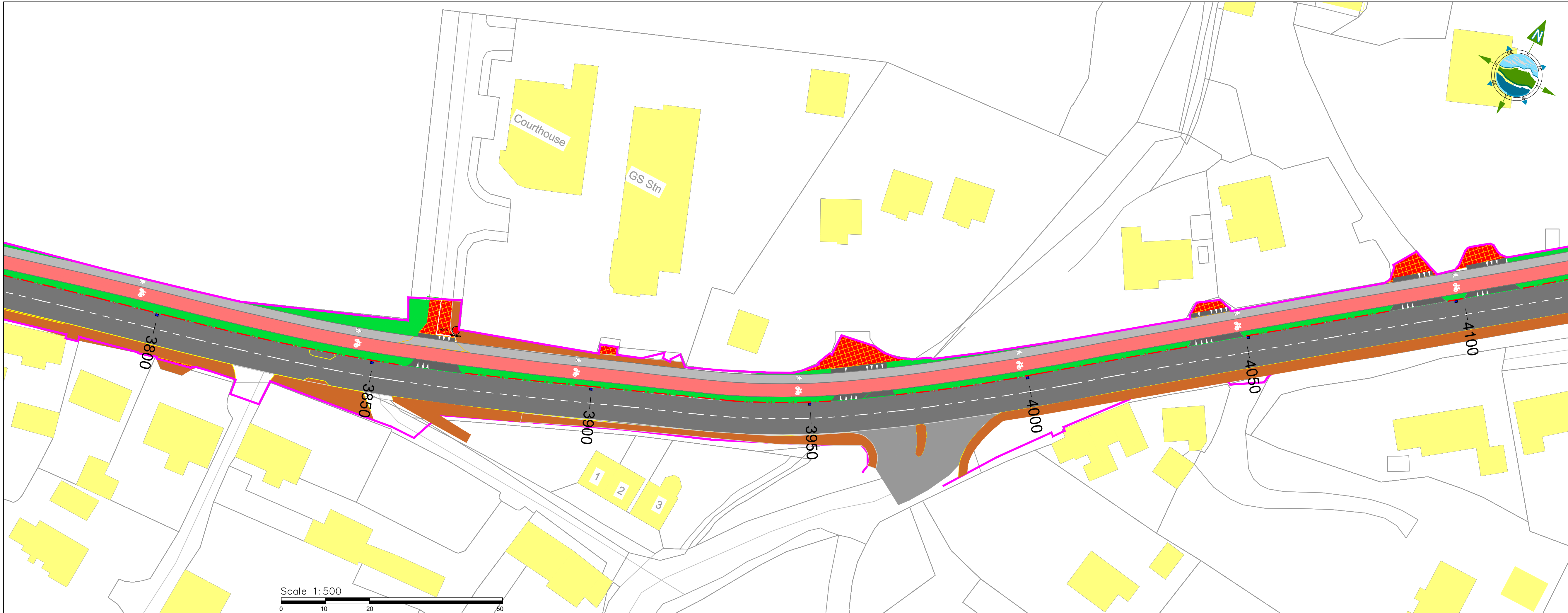
Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon General Arrangement Sheet 07 of 08

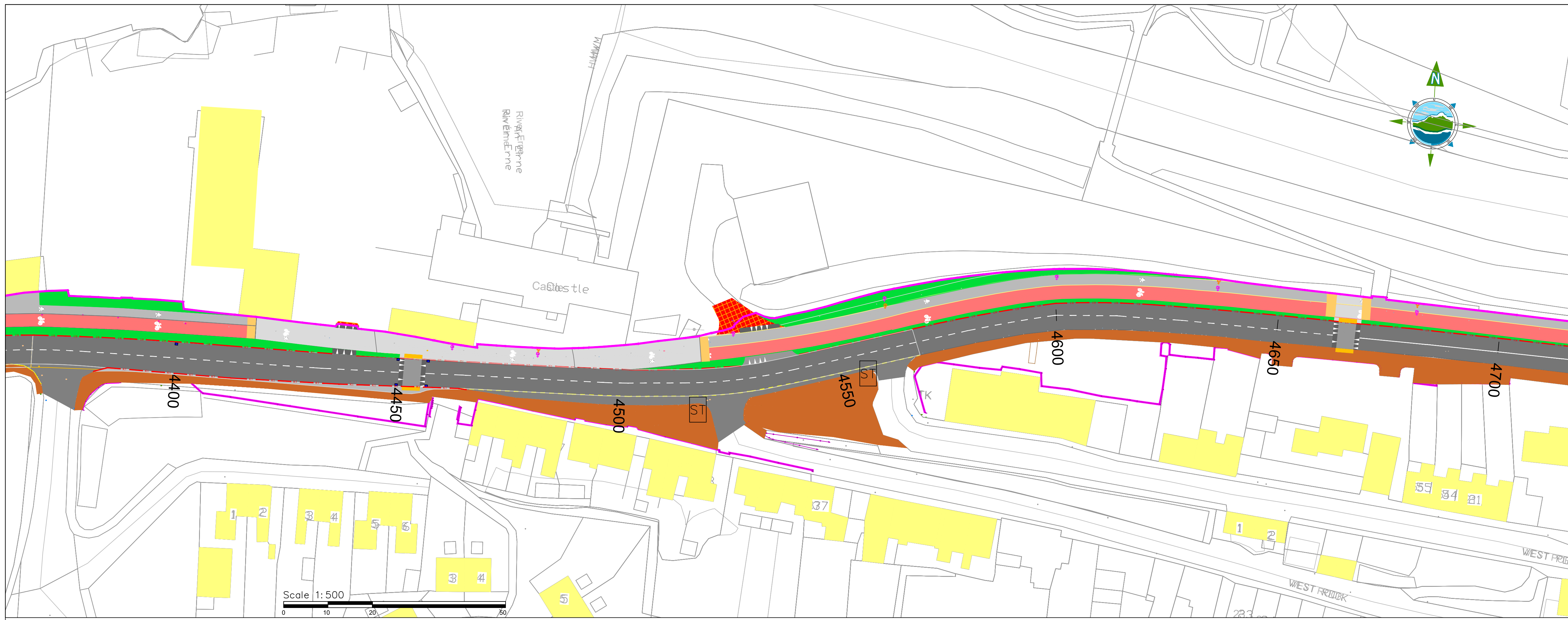
A1 Scale: 1:500 A3 Scale: 1:1000
 Prepared by: CQ Checked: RF Date: 21.07.23
 Project Director: J. O'Flaherty
 Drawing Status: Planning


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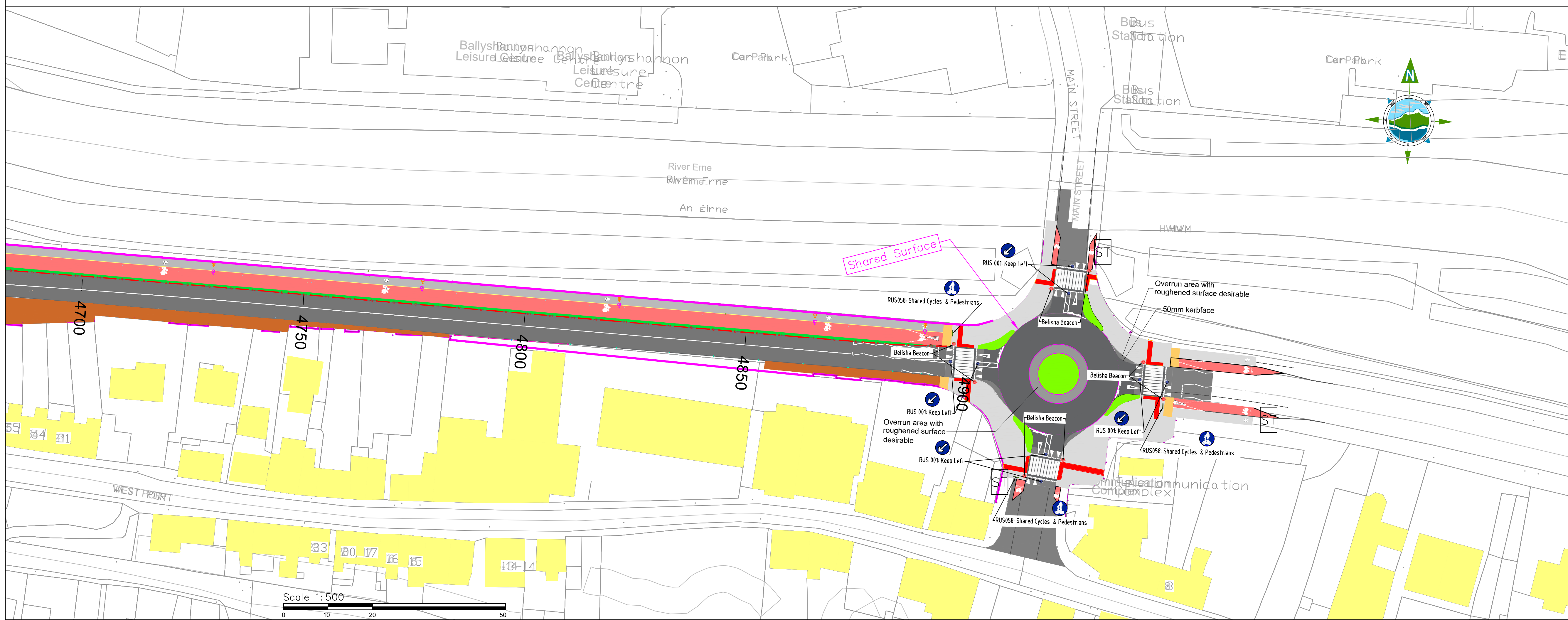


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Project: 11199 Donegal Co Co Active Travel

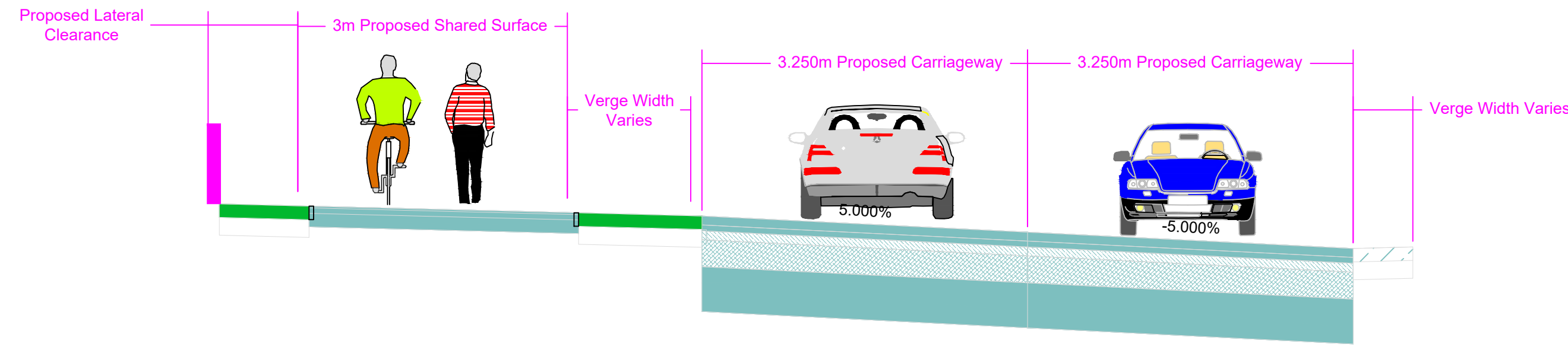
Title: Bundoran to Ballyshannon General Arrangement Sheet 08 of 08

A1 Scale: 1:500	A3 Scale: 1:1000
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Project Director: J. O'Flaherty	
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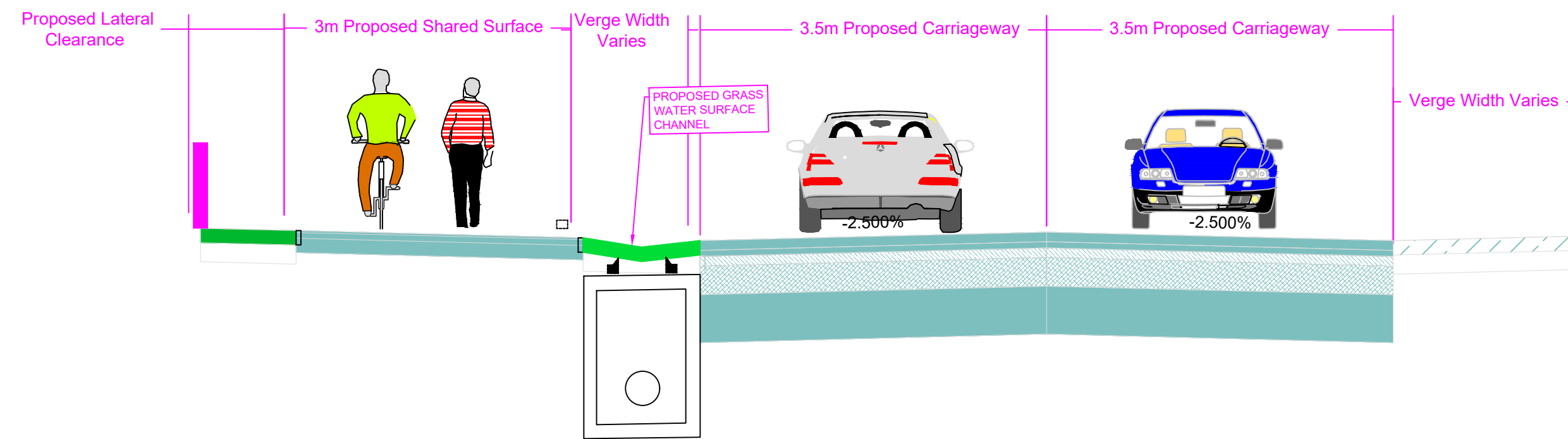
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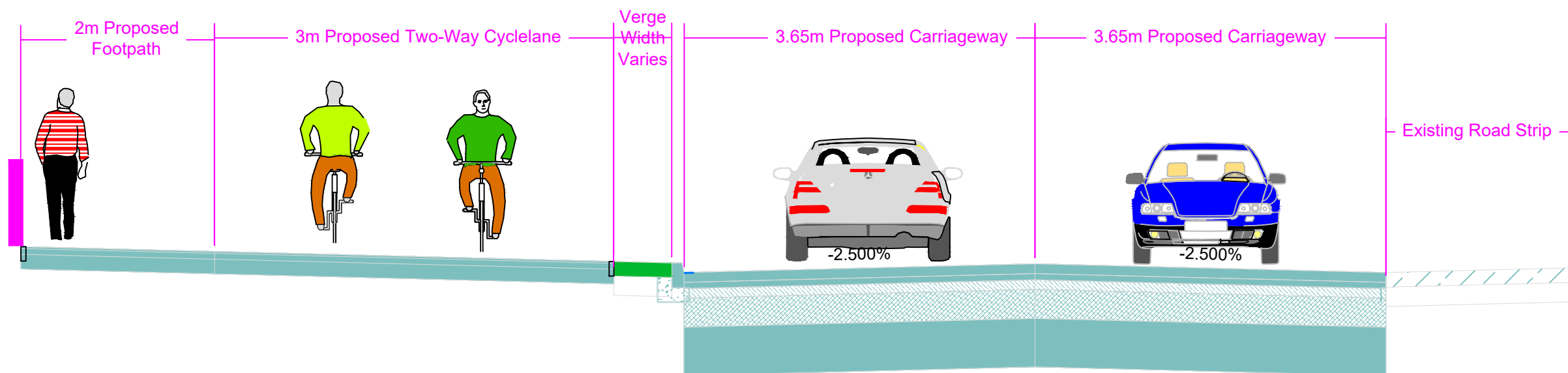
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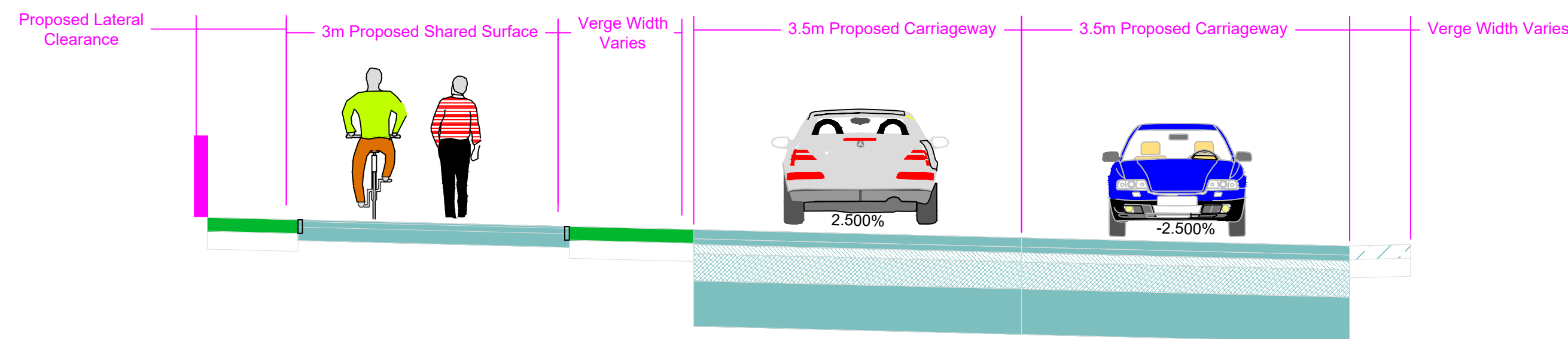
Typical TII Cross-Section with 5% Superelevation



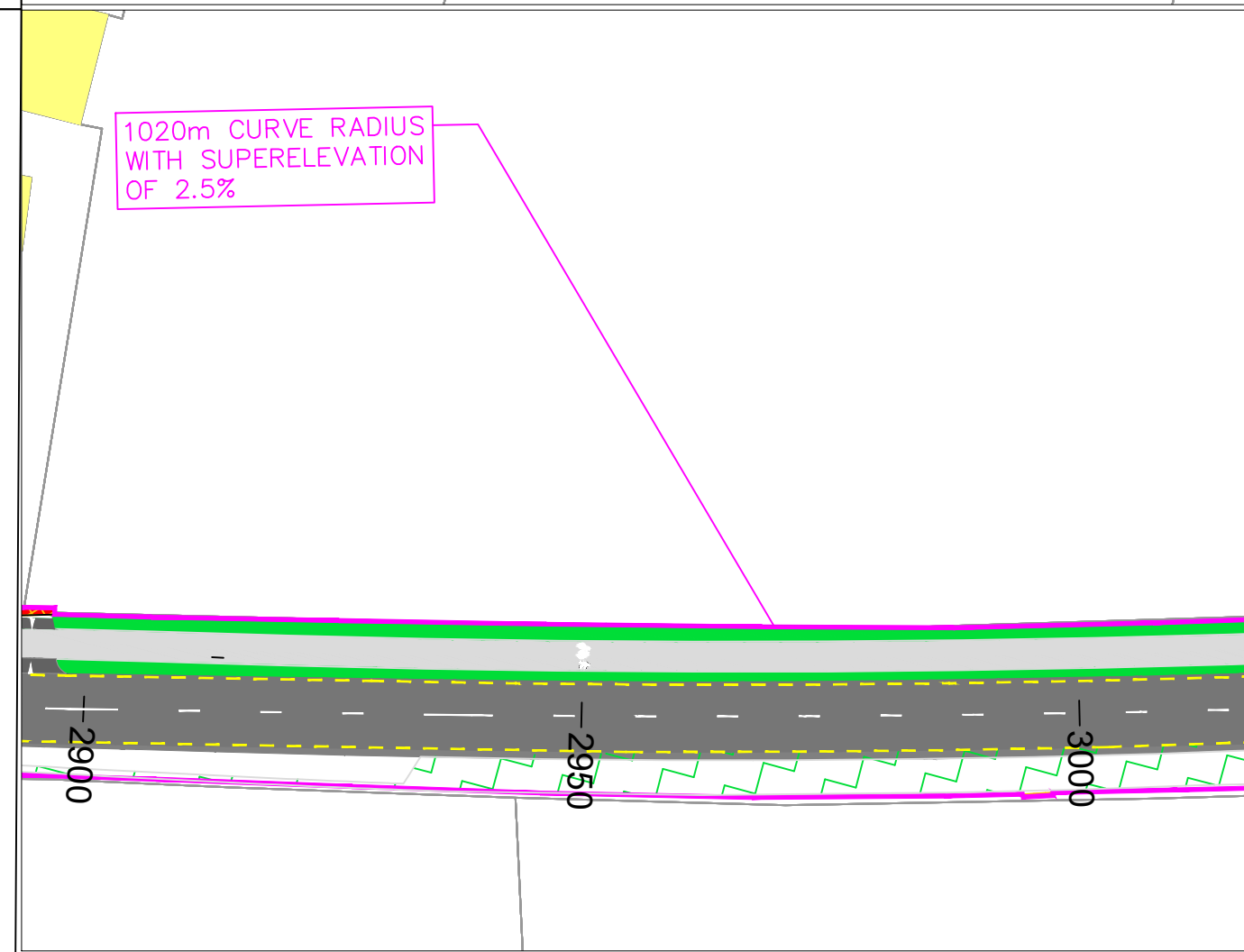
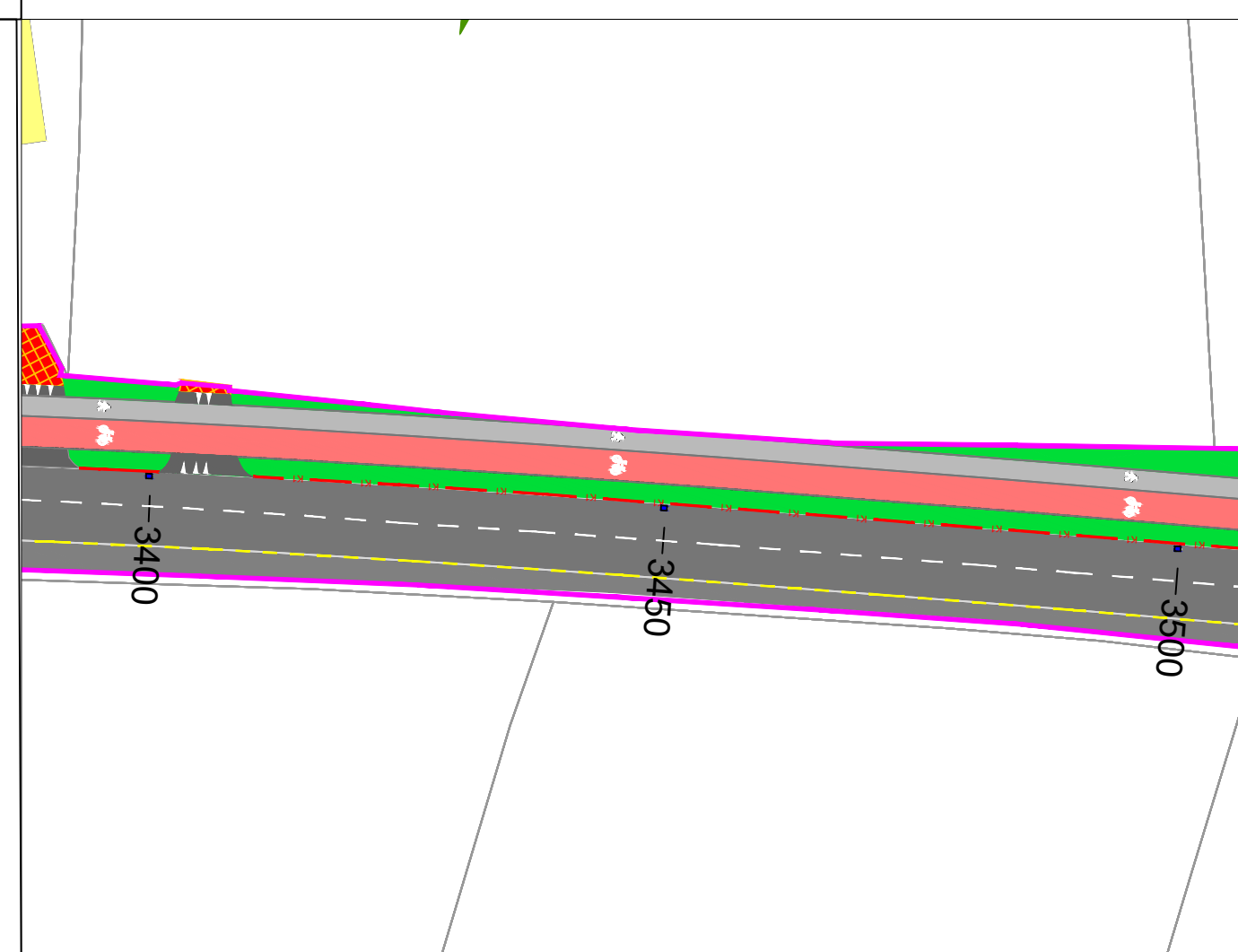
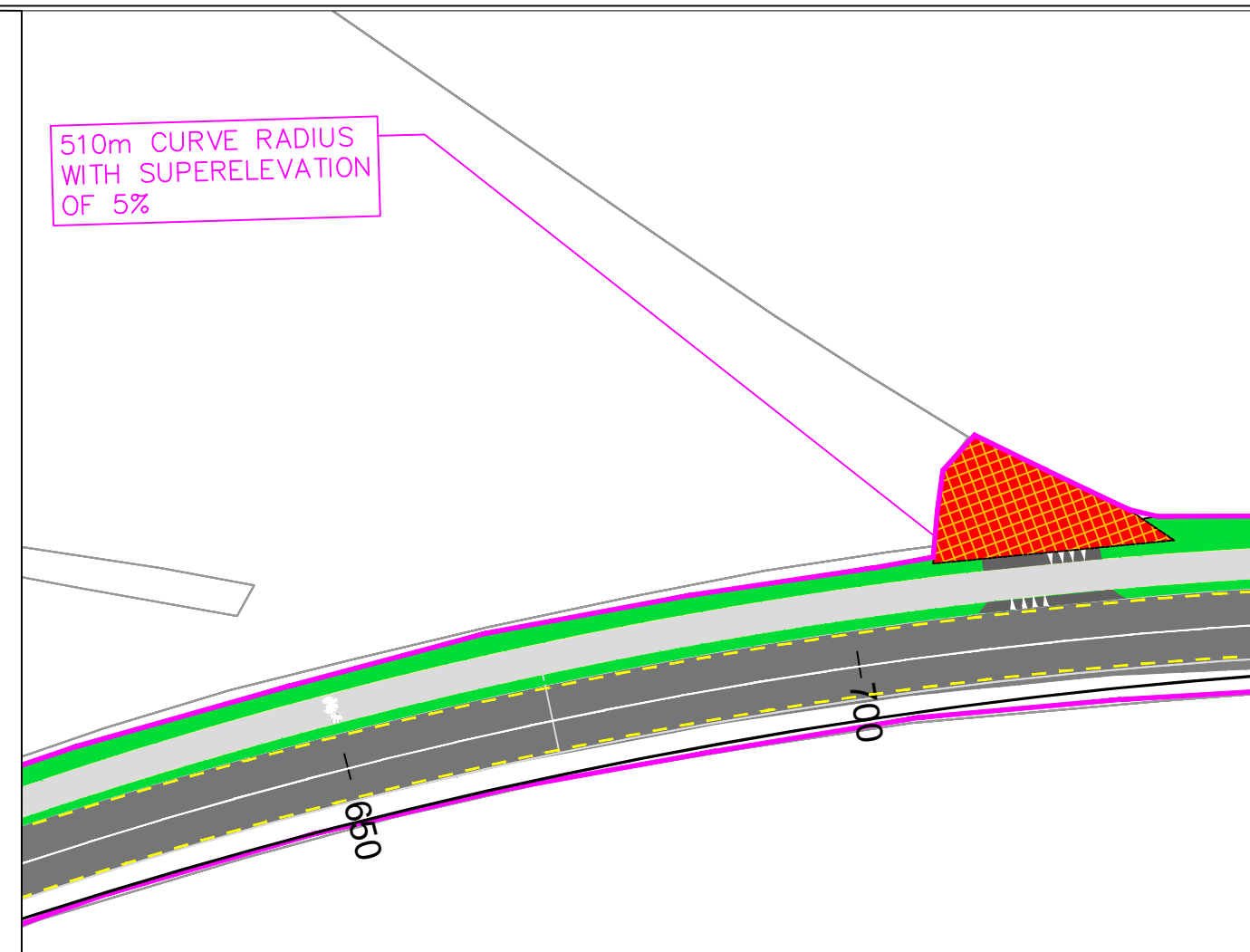
Typical TII Cross-Section



Typical Cross-Section CH3450



Typical TII Cross-Section with 2.5% Superelevation



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C	21.07.23	Planning	CP	RF	JK
B	20.06.23	Draft	CP	RF	JK
A	06.03.23	Draft SD	CP	RF	JK

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: Donegal County Council

Project: 11199 Donegal Co Co Active Travel

Title: Bundoran to Ballyshannon Typical Cross-Sections Sheet 01 of 02

A1 Scale: NTS	A3 Scale: NTS
Prepared by: CQ	Checked: RF
Date: 21.07.23	
Project Director: J. O'Flaherty	
Drawing Status: Planning	

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- EXISTING LOCATION LAMP POST —
- PROPOSED LOCATION LAMP POST —

Rev	Date	Description	By	Chk.	Appr.
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B	20.06.23	Draft	CO	RF	LOK
A	06.03.23	Draft SD	JA	CO	LO

File Name: DCCAT-TOBIN-DR-GA-0001-S0-P01 230620

Client: 
Donegal County Council

Project:
11199 Donegal Co Co Active Travel

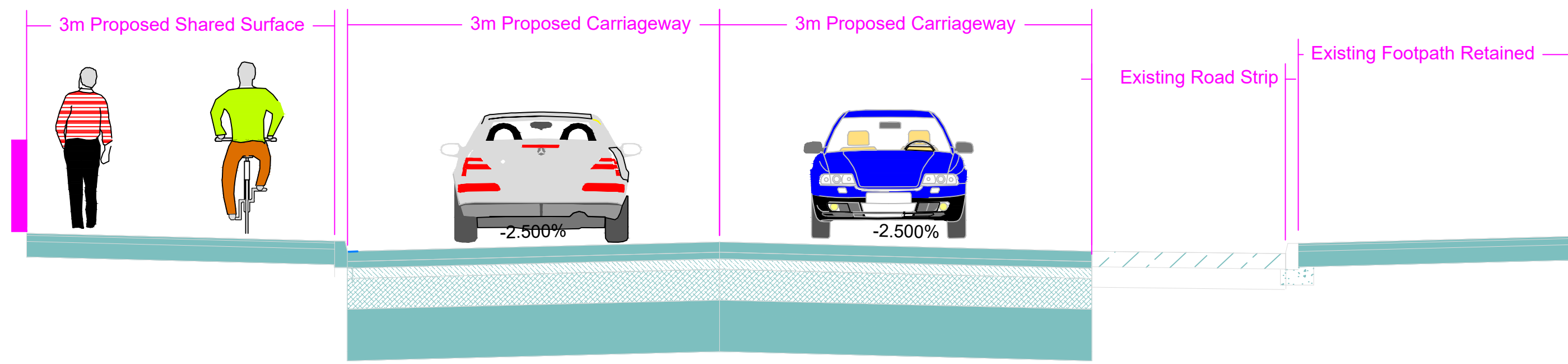
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Bundoran to Ballyshannon
Typical Cross-Sections
Sheet 02 of 02

A1 Scale: NTS	A3 Scale: NTS
Prepared by: CQ	Checked: RF
Date: 21.07.23	
Project Director: J. O'Flaherty	
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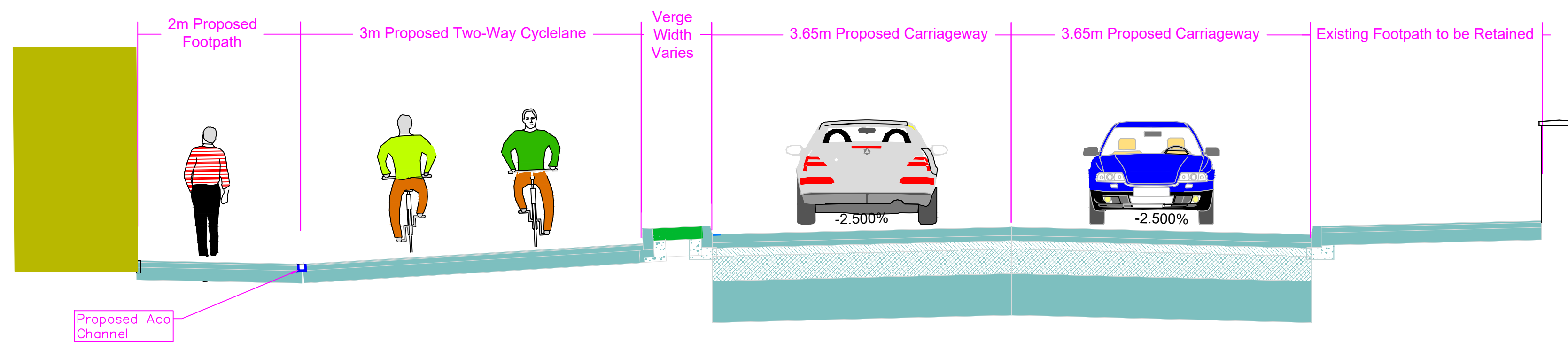


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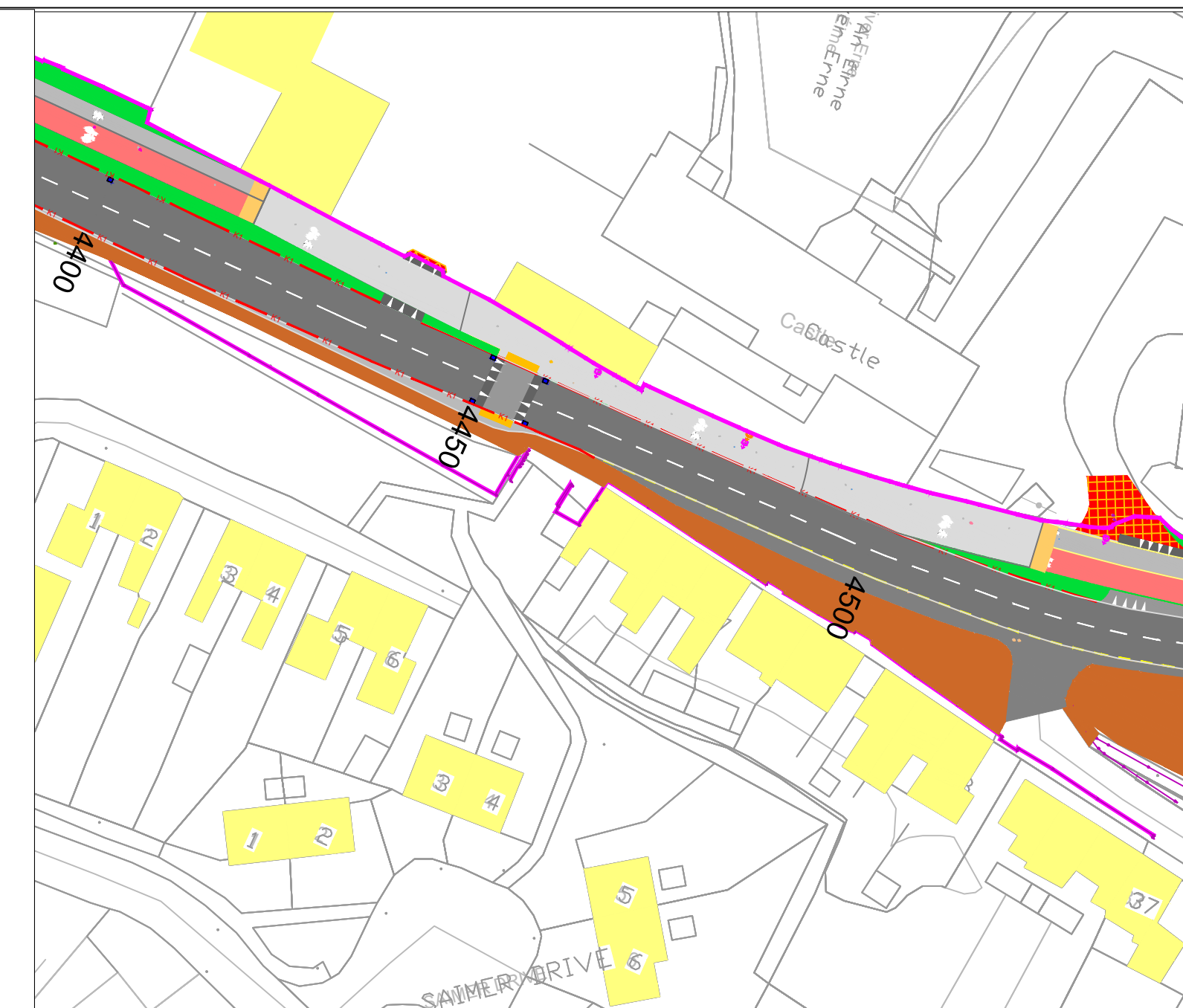
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Typical Cross-Section CH4470



Typical Cross-Section CH4320



Appendix B – Stage 1 Road Safety Audit

TOBIN

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BUILT ON KNOWLEDGE



DONEGAL COUNTY COUNCIL

BUNDORAN TO BALLYSHANNON

STAGE 1 ROAD SAFETY AUDIT



Document Control Sheet

Document Reference	11199 TR08-RSA S1 R267 Bundoran to Ballyshannon
Client:	Donegal County Council
Project Reference	11199-08

Rev	Description	Author	Date	Reviewer	Date	Approval	Date
D01	Draft	MR	22/06/2023	LG	04/07/2023	LG	11/07/2023
A	Final	MR	27/07/2023	LG	27/07/2023	LG	27/07/2023

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1. INTRODUCTION

This report describes the Stage 1 Road Safety Audit carried out on a proposed greenway. The scheme is on the R267, regional road, between Bundoran and Ballyshannon in County Donegal, see Figure 1-1. The extent of the Road Safety Audit is shown in Figure 1-2.

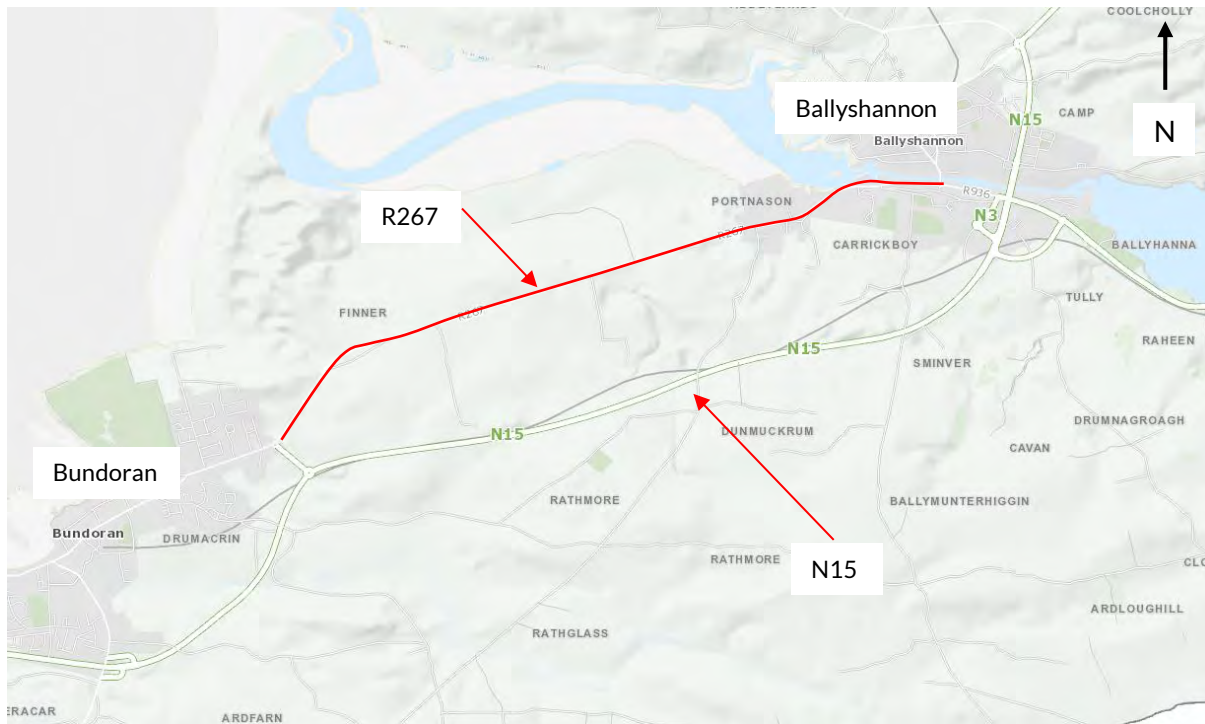


Figure 1-1: Site Location (CYAL50311153 © Ordnance Survey Ireland/Government of Ireland)

1.1 PROPOSED SCHEME

The proposed scheme is approximately 5km long with 1.65km located within the 60km/h speed limit. The scheme consists of a 3.0m wide shared pedestrian / cyclist track within the rural 80km/h speed limit (at chainage 0m to approximant chainage 3,310m). The scheme transitions to a two-way cycle track (4.0m wide) on approach to the roundabout in Ballyshannon, within the 60km/h (chainage approximant 3,310m to the end of scheme). A 2.0m footway is also provided adjacent to the cycle track within the 60km/h speed limit.



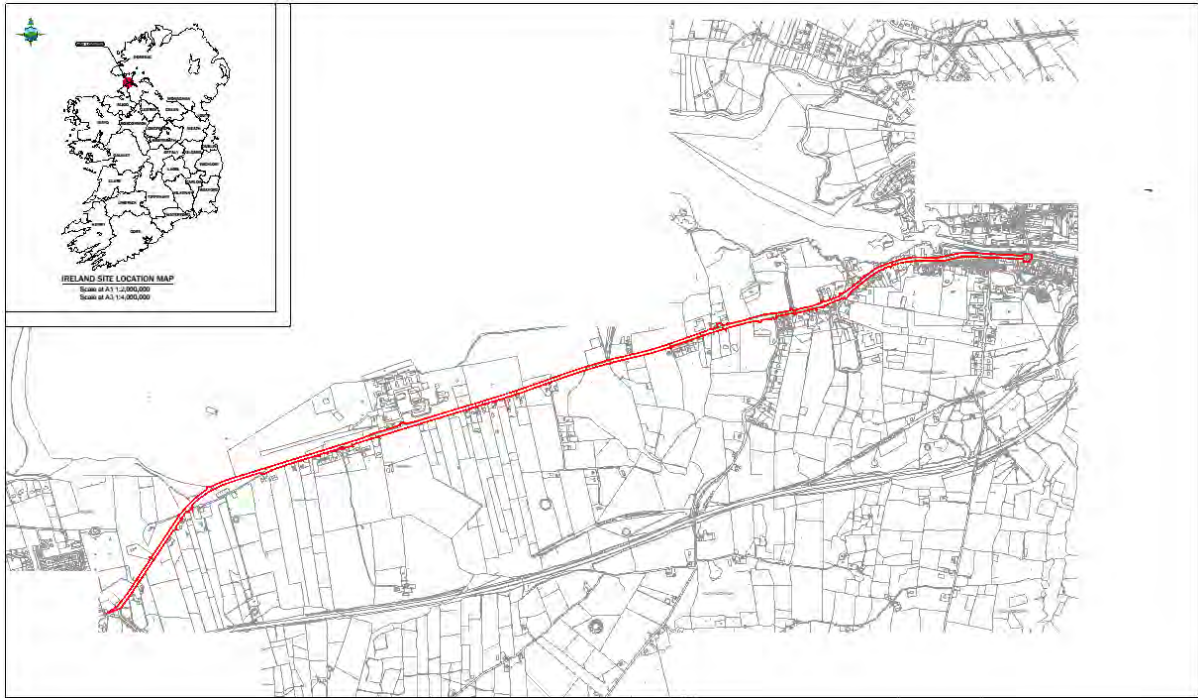


Figure 1-2: Extent of the Road Safety Audit (Drawing 11199-08-1000)

1.2 COLLISION DATA

Collision data has not been supplied with this scheme.

Road Collision Data is not currently available on the Road Safety Authority Database, and therefore the audit team has no access to the historical collision information for this site and / or adjacent roads.

1.3 AUDIT DETAILS

The audit took place in TOBIN Offices in June 2023. The audit comprised an examination of the documents provided by the Design Team and are listed in Appendix A. A site visit was carried out on Wednesday, the 21st of June 2023 between the hours of 13:00-17:30. During the site visit, the weather was sunny, dry and warm, and the road surface was dry.

The Audit Team note that no information was provided on the following:

- Drainage Design;
- Lighting Design;
- Vehicle Tracking;
- Long sections;
- Dimensions along the extent of the scheme; and
- Visibility splays at the junctions

The audit team members were as follows:

Audit Team Leader

- Laura Gaffney – MSc. Env. Eng., BEng (Hons) Civil Eng., CEng., MIEI. Senior Engineer for Roads & Transportation, TOBIN Consulting Engineers – TII Reference LG3386505

Audit Team Member

- Maria Rooney – BEng (Hons) Civil Eng., MEng., CEng., MIEI. Senior Engineer for Roads & Transportation, TOBIN Consulting Engineers. – TII Reference MR3384505

Audit Team Observer

- Rosie Friel – BEng (Hons) Civil Eng. Senior Engineer for Roads & Transportation, TOBIN Consulting Engineers.
- Kevin Duffy-BEng (Hons) Civil Eng., MIEI. Design Engineer for Roads & Transportation, TOBIN Consulting Engineers.

This Stage 1 Audit has been carried out in accordance with the relevant sections of Transport Infrastructure Ireland Publication (Standards) “Road Safety Audit” GE-STY-01024 (December 2017). The team have examined and reported only on the road safety implications of the design submitted and has not examined or verified the compliance of the design to any other criteria. However, to clearly explain a problem or a recommendation, it may be necessary to refer to another Standard or Advice Note, but such reference will not conflict with the requirements of the above Terms of Reference.

The Design Team and Employer (Client) is reminded that the Road Safety Audit Feedback Form, in Appendix C, shall be completed and returned to the Road Safety Audit Team Leader for sign off.



2. ITEMS RESULTING FROM THIS ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 Problem

Road Cross Fall

No cross-sections were provided to the Audit Team and the design does not indicate a pavement resurfacing. On retention of the existing carriageway crossfalls, the crown of the road may result in the running line of the tyre track straddling the crown with the displacement of the kerb line / verge edges. This may cause incorrect vehicles positioned in the road which in turn may result in a head-on collision.

Recommendation

The design team at detailed design should design the vertical road geometry and undertake a Stage 2 Road Safety Audit.

2.1.2 Problem

One-way and Two-way Cycleway Extents

The scheme has both one-way and two-way shared cycleway sections, however, the extent of each section is unclear. The Audit Team are concerned this may cause confusion to the cyclist and pedestrians. This may result in a collision between opposing cyclists with each other and/or with pedestrians.

Recommendation

The design team should provide road markings to indicate if the cycleway is one-way or two-way.

2.1.3 Problem

Existing Lighting Columns

It was observed on site there are existing lighting columns on the R267, however, these have not been incorporated into the proposed scheme. The Audit Team are concerned retaining the existing infrastructure at their current locations may result in hazard for cyclist and/or pedestrians. This may result in a collision with the column or cyclist swerving to avoid the obstruction.





Plate 2-1: Existing Lighting Column

Recommendation

The design team should incorporate all existing lighting columns into the proposed design.

2.1.4 Problem

Existing Infrastructure

It was observed on site, a number of existing road infrastructure chambers / gullies are not incorporated into the proposed scheme. The Audit Team are concerned retaining the existing infrastructure at their current locations may result in level difference and potential slip and trip hazards.



Plate 2-2: Existing Infrastructure



Plate 2-3: Existing Infrastructure



Plate 2-4: Existing Infrastructure

Recommendation

The design team should incorporate all existing infrastructure into the proposed design at detailed design stage.

2.1.5 Problem

Existing Signage

No details were provided in relation to the relocation and/or amendments to the existing signage. The Audit Team are concerned the existing signage may obstruct the cycle way or may not be at the required height for cyclists. This may result in cyclists colliding with the existing signage / swerving to avoid the obstructions.



Plate 2-5: Existing Signage

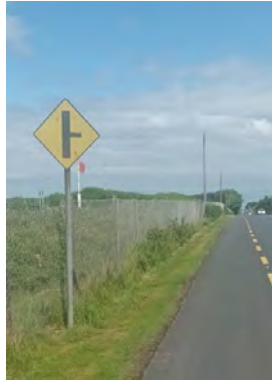


Plate 2-6: Existing Signage



Plate 2-7: Existing Signage

Recommendation

The design team should incorporate all existing signage and proposed signage into the scheme at detailed design, with suitable location and mounting heights.

2.1.6 Problem

Priority at Junctions

The Audit Team noted the absence of road markings and signage at priority junctions. The absence of road markings showing the priority may result in driver confusion and conflict with other road-users.



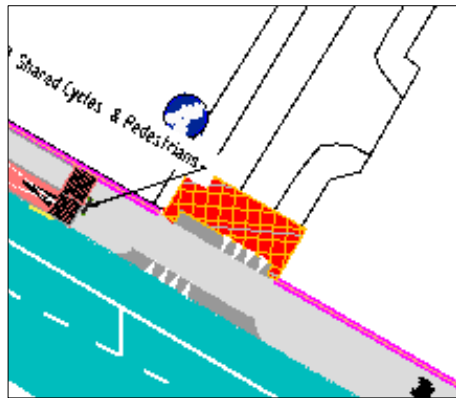


Figure 2-1: Residential Access (Drawing 11199-08-1010-B)

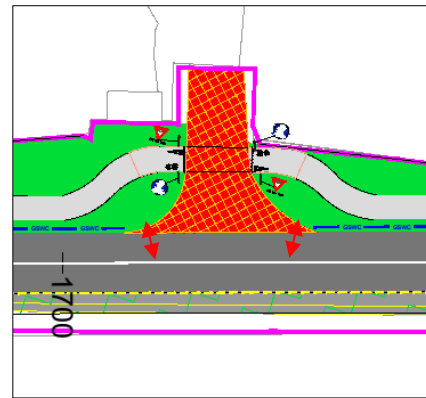


Figure 2-2: Finner Camp Access (Drawing 11199-08-1012-B)

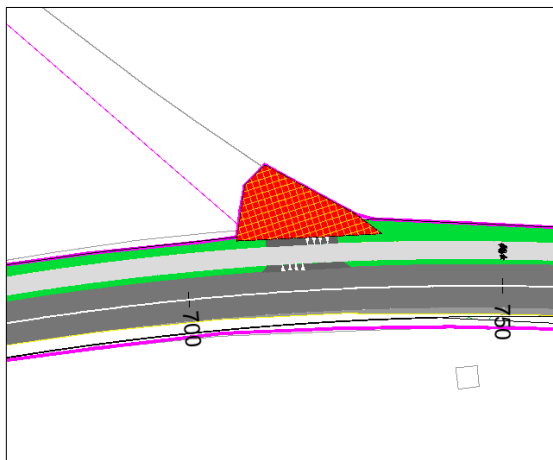


Figure 2-3: Graveyard Access (Drawing 11199-08-1011)

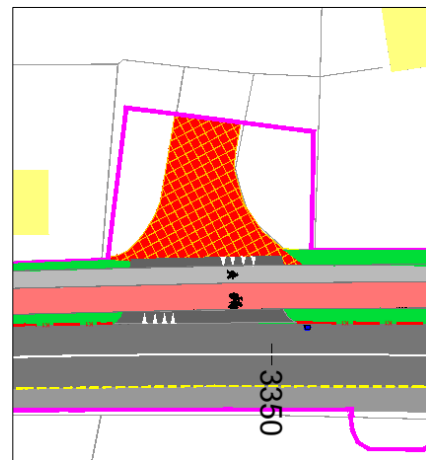


Figure 2-4: Business Park Access (Drawing 11199-08-1015)

Recommendation

The design team should provide road markings to indicate priority and control (i.e. Stop / Yield) at junctions.

2.1.7 Problem

Centreline – Road Marking

The Audit Team noted the centreline for the entire scheme is a continuous solid white line (i.e. no overtaking). The Audit Team are concerned this may lead to driver frustration and unsafe overtaking opportunities. This may result in a head on collision.

Recommendation

The design team should undertake a design and update the road marking along the centreline.



2.2 SCHEME

2.2.1 Problem

Width of Entry Cycle Lane

The Audit Team are concerned the cycle lane width at the start of the scheme and at the Finner Camp are narrow. Cyclists trying to maintain balance while entering the cycle lane or crossing the access may not have enough width and may enter the carriageway. This may result in a collision between a vehicle and cyclist.

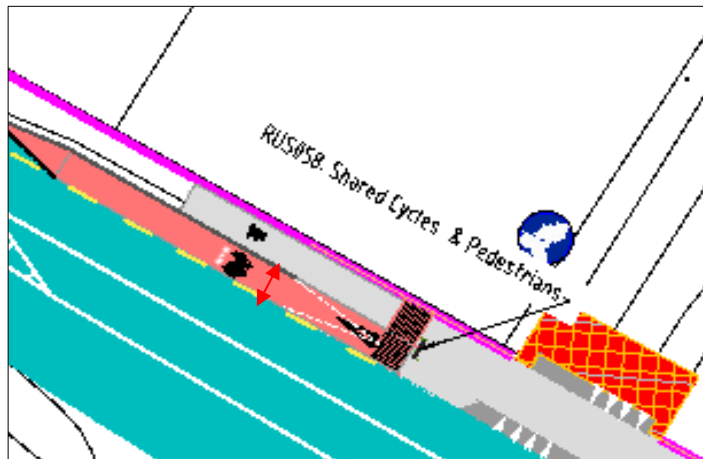


Figure 2-5: Lane width at the start of the scheme
(Drawing 11199-08-1010-B)

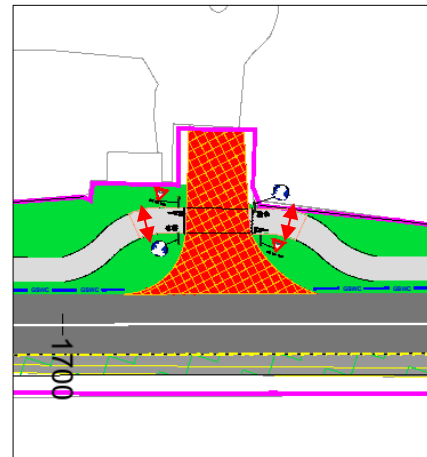


Figure 2-6: Finner Camp Access
(Drawing 11199-08-1012-B)

Recommendation

The design team should provide sufficient “wobble room” for cyclist to keep balance, particularly at lower speeds, approaching junctions, at bends, or on uphill sections.

2.2.2 Problem

Termination of Cycle Lane

The Audit Team noted no warning has been provided to motorists at the termination of the cycle lane where cyclists may be merging with other carriageway traffic. The absence of warning may result in a side-swipe / rear-end collision between a vehicle and a cyclist.



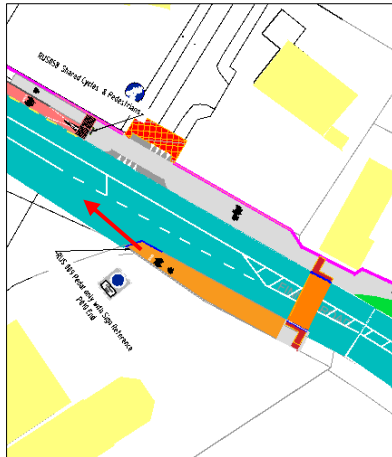


Figure 2-7: Termination of Cycle Lane (Drawing 11199-08-1010-B)

Recommendation

The design team should provide suitable warning to the road users of merging traffic.

2.2.3 Problem

Proposed Crossing

The Audit Team noted the visibility to the right at the proposed crossing is restricted within the visibility spays. The Audit Team are concerned Vulnerable Road Users (VRU) may not be able to see oncoming vehicles or vice versa. This may result in collisions between vehicles and VRU.

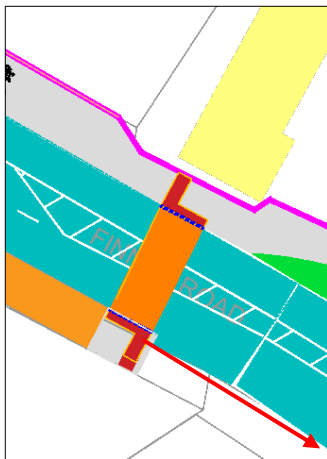


Figure 2-8: Proposed Crossing (Drawing 11199-08-1010-B)



Plate 2-8: Existing Visibility to the right

Recommendation

The design team should provide clear unobstructed visibility spays at all crossings within the scheme.



2.2.4 Problem

Warning Tactile

The Audit Team noted to the north on the crossing no corduroy paving is proposed at the termination of the footway. The Audit Team are concerned visual impaired road users may enter the carriageway. This may result in a collision between a vehicle and visually impaired pedestrians.



Figure 2-9: Absence of Warning Tactiles (Drawing 11199-08-1010-B)

Recommendation

The design team should provide corduroy paving to inform visually impaired road users at the termination of the footway.

2.2.5 Problem

Access onto the Cycle Lane

The Audit Team noted northbound cyclists can access the cycle lane from the roadway through a gap in the grass verge. The concern is cyclists traveling southbound may enter the carriageway towards opposing vehicles. This may result in a collision between a vehicle and a cyclists.



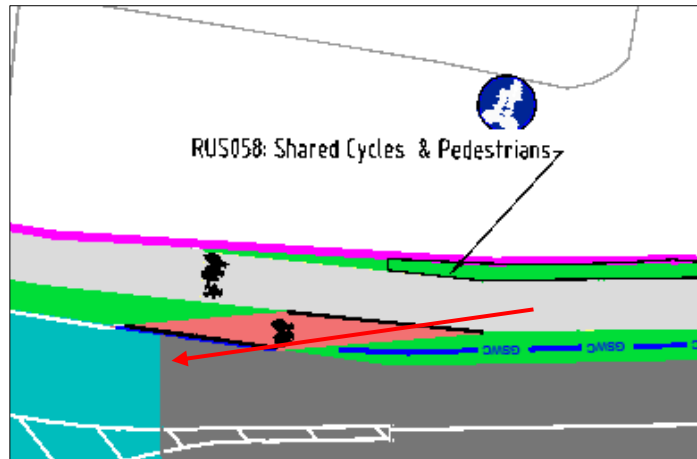


Figure 2-10: Proposed Crossing (Drawing 11199-08-1010-B)

Recommendation

The design team should provide infrastructure to allow both direction of cyclists to depart and re-enter the carriageway in a safe manner.

2.2.6 Problem

Road Studs

The Audit Team noted the existing studs are worn and damaged. The proposed design does not indicate the replacement of the road studs. The concern is vehicles may not be able to determine the road edge/ centre of carriageway / approach to junction during nighttime hours. This may result in collisions between vehicles.



Plate 2-9: Existing Studs



Plate 2-10: Existing Studs

Recommendation

The design team should design the road studs at detailed design.

2.2.7 Problem

Proposed Road Markings

The Audit Team noted that no road edge markings are proposed in rural areas of the scheme. The Audit Team are concerned vehicles may not be able to determine the road edge and veer into the proposed scheme. This may result in a collision between a vehicle and a cyclist.

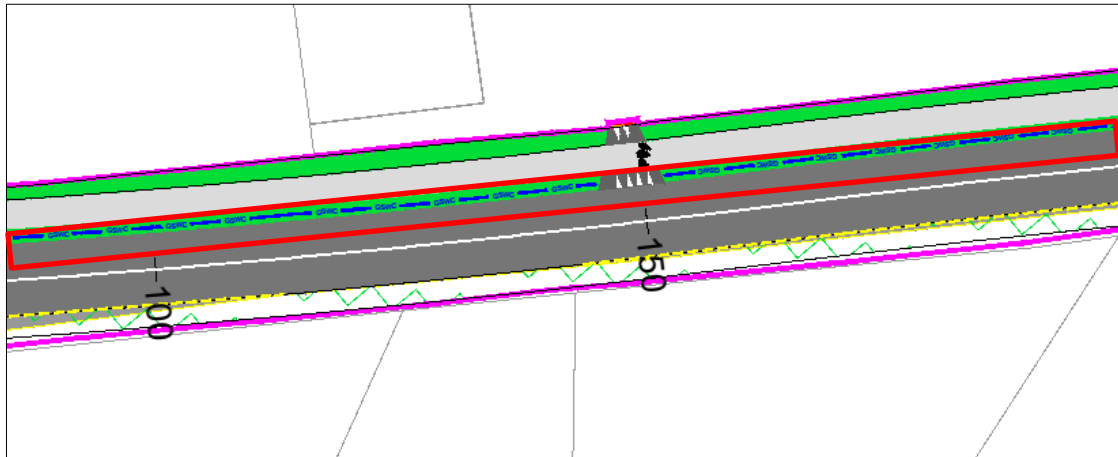


Figure 2-11: Road Marking Design (Drawing 11199-08-1010-B)

Recommendation

The design team should provide suitable road markings in rural areas at detailed design.

2.2.8 Problem

Existing Gate Accesses

The Audit Team noted access gates (at approximate chainage 275m, 525m, 3,300m and 3,400m) which are not incorporated into the design. The concern is the proposed kerbing and level difference may prevent vehicles entering these therefore in order to gain access drivers may increase speed to overcome the proposed kerbing and shared surface. This may result in collisions with between vehicles and VRU.





Plate 2-11: Existing Access (Chainage 275m)



Plate 2-12: Existing Access (Chainage 525m)



Plate 2-13: Existing Access (Chainage 3300m)



Plate 2-14: Existing Access (Chainage 3400m)

Recommendation

The design team should incorporate all existing access into the proposed design.

2.2.9 Problem

Existing Concrete Posts

The Audit Team noted concrete posts along the existing boundary of the scheme. The Audit Team are concerned the concrete posts may be retained and present a roadside hazard in rural areas.





Plate 2-15: Existing Concrete Posts



Plate 2-16: Existing Concrete Posts

Recommendation

The design team should risk assess all roadside hazards on the scheme.

2.2.10 Problem

Height Difference

The Audit Team noted a significant height difference from the existing road level to the neighbouring land. The boundary treatment along this section includes timber post and wire fencing. The Audit Team are concerned the proposed scheme may encroach into the boundary line. The lack of structural support along the extents of this section of the scheme may result in subsidence. Resulting in foundation failure of the proposed scheme and/or cyclist and pedestrians falling into the neighbouring land.



Plate 2-17: Existing Levels

Recommendation

The design team should amend the design to provide a structural support with adequate boundary treatment.



2.2.11 Problem

Drainage

The Audit Team noted the proposed drainage does not tie into any proposed outlet. The Audit Team are concerned this may lead to ponding on the surface water. This may result in vehicles aquaplaning.

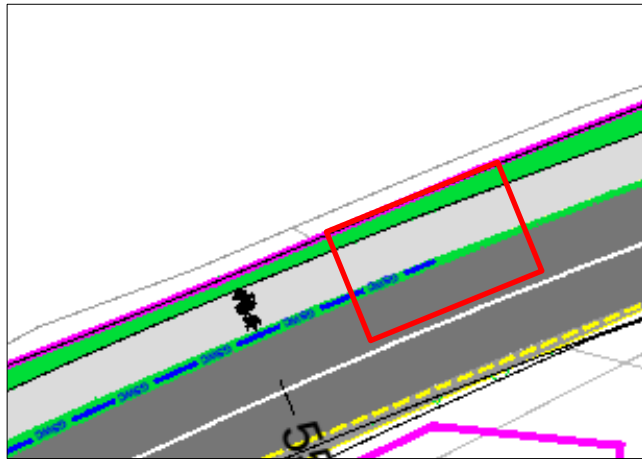


Figure 2-12: Proposed Drainage
(Drawing 11199-08-1011)



Figure 2-13: Proposed Drainage
(Drawing 11199-08-1011)

Recommendation

The design team should at detailed design provide suitable outfalls from proposed drainage.

2.2.12 Problem

Graveyard Junction

The Audit Team noted the visibility to the right of the Graveyard Junction is obstructed. The Audit Team are concerned drivers may not be able to see oncoming vehicles, resulting in vehicles edging out in conflict with pedestrian / cyclist / vehicles. .



Plate 2-18: Existing Visibility

Recommendation



The design team should provide clear unobstructed visibility splays at all entrance / junctions on the scheme.

2.2.13 Problem

Available Width for Scheme

It was observed on site the available width for the proposed scheme is narrow along approximate chainages 1,350m and 2,350m. The Audit Team are concerned the scheme may narrow for an extended length which may result in cyclists or pedestrian entering the carriageway. This may result in a collision between pedestrian/ cyclist and a vehicle.



Plate 2-19: Existing Verge Width (approx. 1350m)



Plate 2-20: Existing Verge Width (approx. 2350m)

Recommendation

The design team should provide the required standard width for the extent of the scheme or modify the design to facilitate the constrained areas.

2.2.14 Problem

Proximity to Roadway

It was observed the cycle lane is adjacent to the road edge in areas where there is available space to provide greater separation between the motorists and VRUs. The Audit Team observed onsite, parking within the newly designed cycleway. The Audit Team are concerned vehicles may cross the cycleway to park in the area between the cycleway and boundary. This may result in a collision between pedestrian/cyclist and a vehicle.



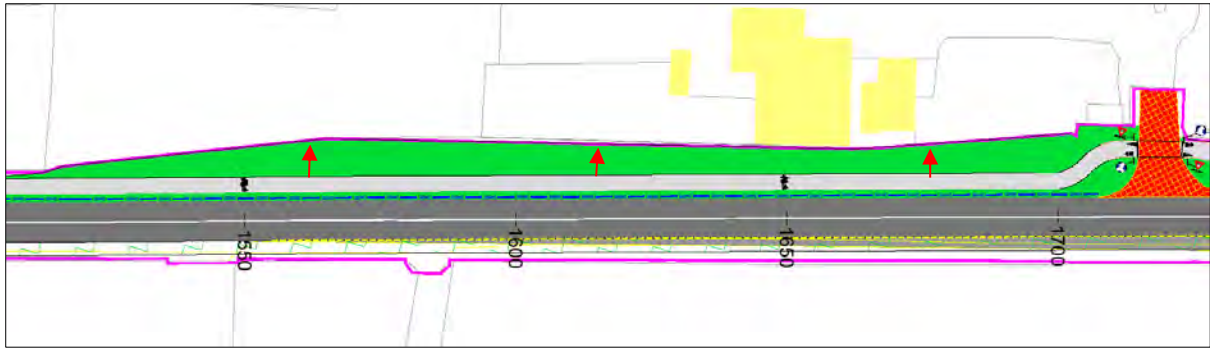


Figure 2-14: Proposed Layout (Drawing 11199-08-1012)

Recommendation

The design team should provide separation where available between cyclists and motorists.

2.2.15 Problem

Existing Pedestrian Access

It was observed the existing pedestrian access at Finner Camp has not been incorporated into the design. This may result in pedestrians accessing the Camp via the paved vehicular access in conflict with motorists.



Plate 2-21: Existing Pedestrian Access

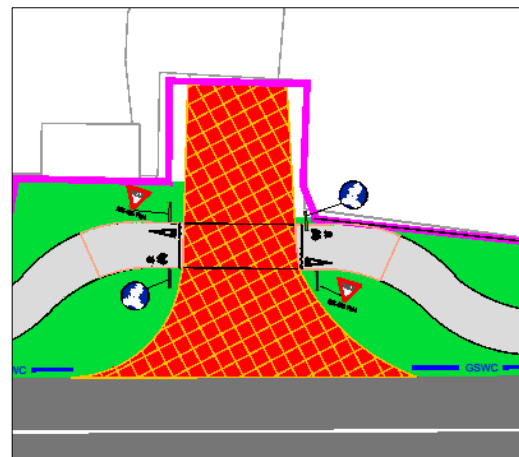


Figure 2-15: Proposed Works (Drawing 11199-08-1012)

Recommendation

The design team should incorporate the existing pedestrian access into the design.

2.2.16 Problem

Start of Two-way cycleway

The Audit Team note the no information has been provided regarding the start of the two-way of the cycle lane. The Audit Team are concerned this may lead to road user confusion. This may lead to cyclists' collisions.



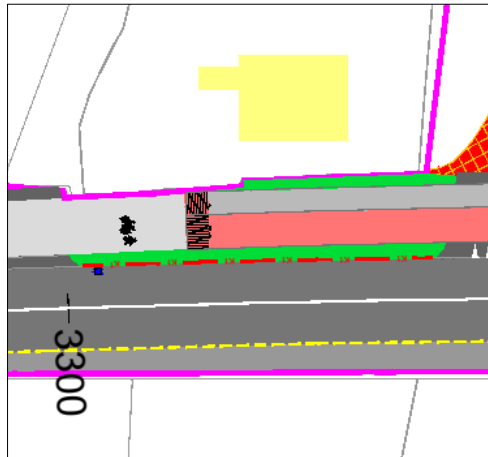


Figure 2-16: Start of Two-way cycleway (Drawing 11199-08-1017)

Recommendation

The design team should provide signage / marking to inform users of the start of the two-way section.

2.2.17 Problem

Business Park Access

The Audit Team noted the visibility to the right of the business park access is obstructed. The Audit Team are concerned drivers may not be able to see oncoming cyclists. Due to the steep downward gradient, cyclist may be traveling at fast speeds and may not be visible to motorists exiting the Business Park. This may result in a collision between a cyclist and a vehicle.



Plate 2-22: Existing Visibility

Recommendation

The design team should provide clear unobstructed visibility splays.

2.2.18 Problem

Width at the mouth of Junction

The Audit Team noted the mouth of the two entrances (Business Park and housing estate at Chainage 3,950) is very wide. The Audit Team are concerned that pedestrians crossing this junction may be a long time in conflict with turning vehicles. This may result in a collision between a vehicle and pedestrian.

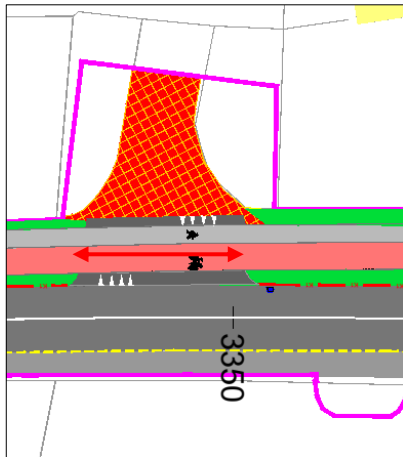


Figure 2-17: Business Park Entrance
(Drawing 11199-08-1015)

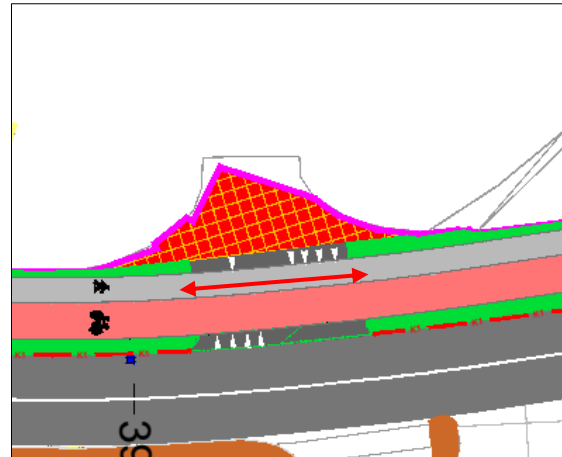


Figure 2-18: Private Housing Estate
(Drawing 11199-08-1016)

Recommendation

The design team should design the entrances in accordance with the current urban standards with tighter junction for improved pedestrian facilities.

2.2.19 Problem

Connectivity

The Audit Team noted no pedestrian crossing facilities for those with mobility / visual impairments are proposed over an extended length of approximately 800m between chainage 3,650m and 4,450m. The Audit Team are concerned pedestrians desire lines exist from the residential development on the southern side to the views on the northern side of the carriageway. This lack of facility may result in a pedestrian / VRU becoming stranded in the carriageway and result in a collision with a vehicle.

Recommendation

The design team should provide a means for pedestrians with visual / mobility impairments to cross the carriageway.



2.2.20 Problem

Current Height Variations

The Audit Team observed an existing elevation difference between the footway and carriageway (i.e. chainage 4,300m-4,350m) to allow access to the existing dwellings. The roadway is narrowed in the design at this location, to accommodate a wider segregated cycle lane and footway with grass separation to the carriageway. The Audit Team are concerned that no change in gradient is proposed at the cycleway at this location. This may result in a trip by pedestrians stepping out up onto the new footway level and drainage issues at these houses at lower gradients than the road and cycleway.



Plate 2-23: Exiting Levels

Recommendation

The design team should upgrade the design at this location at detailed design, taking into consideration the level difference on road users, cyclists, pedestrians, and potential residents.

2.2.21 Problem

Proposed Crossing

The Audit Team noted the visibility to the left at the proposed crossing is restricted within the visibility spays. The Audit Team are concerned VRU may not be able to see oncoming vehicles or vice versa. This may result in a collision between a vehicle and cyclist.



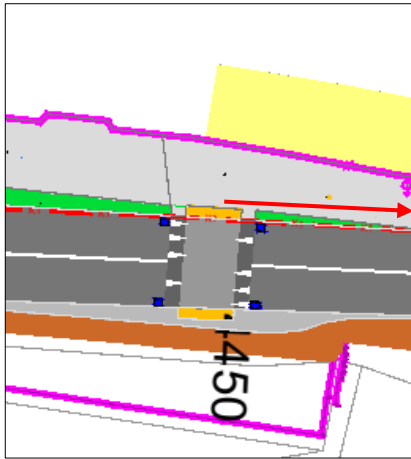


Figure 2-19: Proposed Crossing
(Drawing 11199-08-1017)



Plate 2-24 Visibility to the Left

Recommendation

The design team should provide clear unobstructed visibility splays.



2.2.22 Problem

Proposed Crossing

The Audit Team noted the crossing at chainage 4,525m does not have tactile paving on the opposing footway. The Audit Team are concerned visual impaired road users may not be directed to a safe access onto the footway may be struck by a vehicle.



Figure 2-20: Proposed Crossing (Drawing 11199-08-1017)

Recommendation

The design team should direction for visual impaired road users access onto the footway..

2.2.23 Problem

Existing Bollards

The Audit Team noted existing bollards has not been reinstated along the proposed scheme. The Audit Team are concerned vehicles, may park along the proposed scheme and this may result in VRU entering the carriageway. It was advised to the Audit Team that during GAA matches, parking was previously observed at this location. This may result in pedestrians entering into the carriageway in conflict with vehicles when the footway is obstructed.



Figure 2-21: Proposed Crossing (Drawing 11199-08-1017)

Recommendation

The design team should reinstate the bollards / provide alternative physical feature.

2.2.27 Problem

Vehicles Manoeuvres

The Audit Team observed a bus station north of the roundabout. The concern is the proposed roundabout may not have sufficient available width (i.e. horizontal geometry) for a bus turning manoeuvre. This may result in buses entering the footway to make the manoeuvre resulting in a collision between a bus and a VRU.

Recommendation

The design team should carry out an auto track for a bus to ensure a turning manoeuvre is possible.



3. OBSERVATIONS

3.1 OBSERVATION

Existing Junction

The Audit Team noted a STOP sign has is not provided at the existing junction.



Plate 3-1: Existing Finner Avenue Junction

3.2 OBSERVATION

Existing Warning Sign

The Audit Team noted the existing warning signage on approach to the roundabout is worn.



Plate 3-2: Existing Warning Signage



3.3 OBSERVATION

Existing Warning Sign

The Audit Team noted only one speed limit signage is provided at the start of the 80km/h speed limit terminal.



Plate 3-3: Existing Speed Limit Signs

3.4 OBSERVATION

Existing Warning Sign

The Audit Team observed the sign on the splitter island has been struck and not replaced.



Plate 3-4: Existing Speed Limit Signs

3.5 OBSERVATION

Existing Warning Sign

The Audit Team observed a temporary plate has been used to cover an access chamber which has been left as a permanent measure.





Plate 3-5: Existing Access Chamber

3.6 OBSERVATION

Poor Pavement

The Audit Team observed this section of the scheme has very poor pavement.



Plate 3-6: Poor Pavement

3.7 OBSERVATION

Redundant Post

The Audit Team observed redundant posts on the R267.



Plate 3-7: Redundant Posts



3.8 OBSERVATION

Existing Junction

The Audit Team noted the mouth of the existing junction is very wide. The Audit Team are concerned that pedestrians crossing this junction may spend a long time in the carriageway and in conflict with turning vehicles.



Plate 3-8: Existing Access

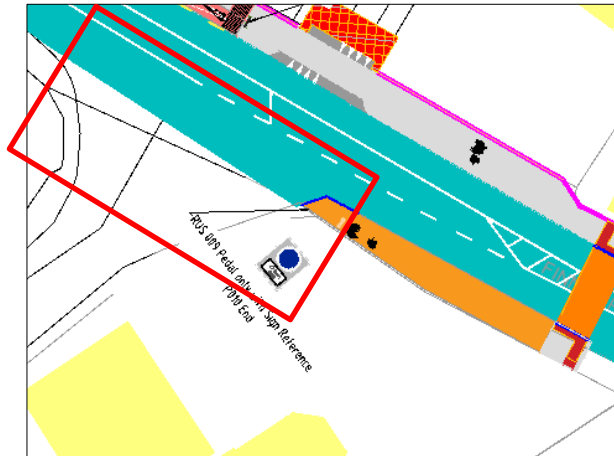


Figure 3-1: Proposed works at junction
(Drawing 11199-08-1010-B)

3.9 OSERVATION

Proposed Drainage

The Audit Team noted a surface water channel is proposed along the grass verge. The Audit Team are concerned if inadequate gradient it may present a roadside hazard. This may result in loss of control of a vehicle.

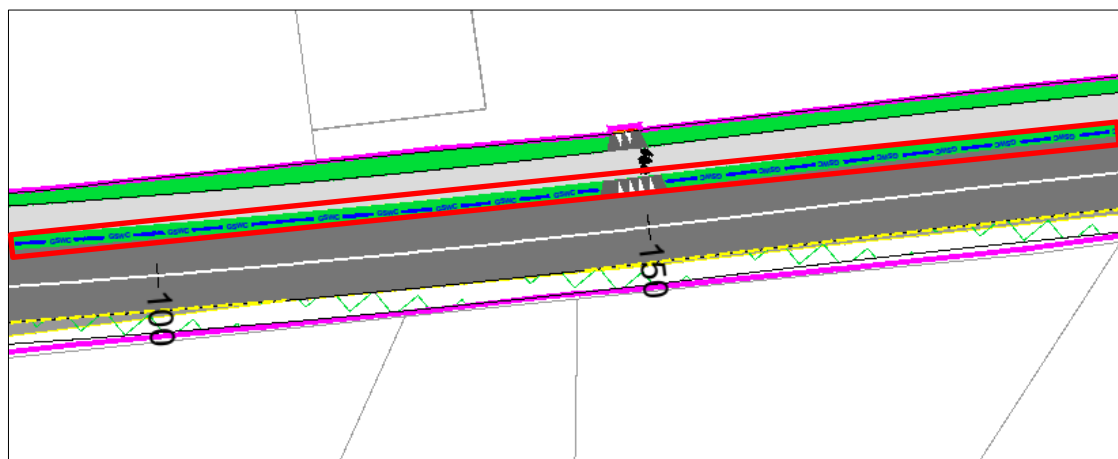


Figure 3-2: Road Drainage (Drawing 11199-08-1010-B)

Appendix A LIST OF DOCUMENTS EXAMINED

11199-1012-B Sheet 01 of 08

11199-1013-B Sheet 02 of 08

11199-1014-B Sheet 03 of 08

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11199-1016-B Sheet 05 of 08

11199-1017-B Sheet 06 of 08

11199-1018-B Sheet 07 of 08

11199-1019-B Sheet 08 of 08

11199-1020-B Sheet 01 of 02

11199-1021-B Sheet 02 of 02



Appendix B RSA TEAM APPROVAL BY TII



Laura Gaffney

From: TII Systems Notification <noreply@tii.systems>
Sent: Tuesday 4 May 2021 18:05
To: Laura Gaffney
Cc: roadsafetyaudits@nra.ie; Fiona.Bohane@corkrdo.ie; Alastair.DeBeer@TII.ie; Bryan.kennedy@TII.ie; LCurtis@Kerrycoco.ie
Subject: RSAAS - Road Safety Audit Approvals System - Auditor Approval LG3386505

Importance: High

Follow Up Flag: Follow up
Flag Status: Flagged

*Laura Gaffney
Fairgreen House
Fairgreen Road
Galway
H91 AXK8*

Date: 04/05/2021

Ref: LG3386505

re: APPROVAL AS ROAD SAFETY AUDITOR

Dear Laura Gaffney,

You meet the qualification and experience requirements for Road Safety Audit as follows:

Scheme Category	Audit Team Status	Team Leader Expiry Date
Road Scheme	Team Leader	30/09/2023
Development Scheme	Team Leader	30/09/2023

The above assessment is based on information supplied and the qualification and experience requirements of National Roads Authority in accordance with HD 19 "Road Safety Audit". Further approval through RSAAS must be sought for the proposed road safety audit team for each audit undertaken on a National Road.

Yours sincerely,

Lucy Curtis

Regional Road Safety Engineer
roadsafetyaudits@tii.ie

Laura Gaffney

From: TII Systems Notification <noreply@tii.systems>
Sent: Thursday 30 May 2019 16:28
To: Maria Rooney
Cc: roadsafetyaudits@nra.ie; Fiona.Bohane@corkrdo.ie; Alastair.DeBeer@TII.ie; ronan.quinn@tii.ie; nicholas.mcdonnell@tii.ie
Subject: RSAAS - Road Safety Audit Approvals System - Auditor Approval MR3384505
Importance: High

*Maria Rooney
Fairgreen House
Fairgreen Road
Galway
H91 AXK8*

Date: 30/05/2019

Ref: MR3384505

re: APPROVAL AS ROAD SAFETY AUDITOR

Dear Maria Rooney,

You meet the qualification and experience requirements for Road Safety Audit as follows:

Scheme Category	Audit Team Status	Team Leader Expiry Date
Road Scheme	Team Member	
Development Scheme	Team Member	

The above assessment is based on information supplied and the qualification and experience requirements of National Roads Authority in accordance with HD 19 "Road Safety Audit". Further approval through RSAAS must be sought for the proposed road safety audit team for each audit undertaken on a National Road.

Yours sincerely,

Lucy Curtis

Regional Road Safety Engineer
roadsafetyaudits@nra.ie

Appendix C ROAD SAFETY AUDIT FEEDBACK FORM



Road Safety Audit Feedback Form

Scheme: Bundoran To Ballyshannon

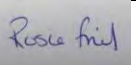

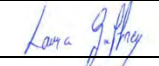
Audit Stage: 1

Route No.: R267 Regional Road

Date of Audit: 12/07/2023

To be Completed by Designer				To Be Completed by Audit Team Leader
Paragraph No. in Safety Audit Report	Problem accepted (yes/no)	Recommended Measures Accepted (yes/no)	Alternative Measures (describe). Give reason for not accepting recommended measure	Alternative Measures or reasons accepted by auditors(yes/no)
2.1.1	Y	Y		
2.1.2	Y	Y		
2.1.3	Y	Y		
2.1.4	Y	Y		
2.1.5	Y	Y		
2.1.6	Y	Y		
2.1.7	Y	Y		
2.2.1	Y	Y		
2.2.2	Y	Y		
2.2.3	Y	Y		
2.2.4	Y	Y		
2.2.5	Y	Y		
2.2.6	Y	Y		
2.2.7	Y	Y		
2.2.8	Y	Y		
2.2.9	Y	N	Risk assessment will be undertaken at Detail Design Stage	Yes
2.2.10	Y	Y		
2.2.11	Y	Y		
2.2.12	Y	Y		
2.2.13	Y	Y		
2.2.14	Y	Y		

To be Completed by Designer				To Be Completed by Audit Team Leader
Paragraph No. in Safety Audit Report	Problem accepted (yes/no)	Recommended Measures Accepted (yes/no)	Alternative Measures (describe). Give reason for not accepting recommended measure	Alternative Measures or reasons accepted by auditors(yes/no)
2.2.15	Y	Y		
2.2.16	Y	Y		
2.2.17	Y	Y		
2.2.18	Y	Y		
2.2.19	Y	Y		
2.2.20	Y	Y		
2.2.21	Y	Y		
2.2.22	Y	Y		
2.2.23	Y	y		
2.2.24	Y	Y		
2.2.25	Y	Y		
2.2.26	Y	Y		
2.2.27	Y	Y		

Signed:		Designer	Rosie Friel	Date:	19 th July, 2023
Signed:		Client	James Curran	Date:	19 th July 2023
Signed:		Audit Team Leader	Laura Gaffney	Date:	27/07/23



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Appendix C – EIA Screening Report

Donegal County Council



Roads & Transportation Directorate

PLANNING AND DEVELOPMENT REGULATIONS 2001
(as amended)

Environmental Impact Assessment

Preliminary Examination

in accordance with Article 120 of the above regulations.

Donegal Municipal District Active Travel Projects

R267 Bundoran to Ballyshannon

Donegal County Council
Road Design Office
County House
Lifford
Co. Donegal
Feb 2023

STAGE 1.a – EIA PRE-SCREENING

Case Ref:	R 267 Bundoran to Ballyshannon Active Travel Projects		
Development Summary	<p>Donegal County Council proposes to provide a cycle and pedestrian corridor between Bundoran and Ballyshannon Co. Donegal including narrowing of the R267 along the route to facilitate the new cycle and pedestrian route.</p> <p>The scheme proposes to provide a secure, connected active travel network that works for all members of the community that can be used safely and enjoyed by all.</p> <p>The project will include:</p> <ol style="list-style-type: none"> a) New continuous 3m shared cycle and pedestrian route along the R267 from 50m west of the Bundoran Roundabout to the 60kph speed limit in Ballyshannon. b) New 2m wide footway and 3m 2-way cycle track from the 60kph speed limit signs to the Ballyshannon roundabout c) Provision of new pedestrian crossings on; <ul style="list-style-type: none"> • the R267 west of Finner road roundabout • R267 at 2 locations within the speed limits of Ballyshannon town d) Upgrading the existing roundabout in Ballyshannon. e) New road markings and signage on cycle and pedestrian route to ensure that the residents, drivers and tourists alike fully understand the new cycle and pedestrian route and the new road layout. f) New road markings, signage, drainage works, landscaping, and accommodation works. g) New road layout with kerbing for the new cycle way and footpaths to be built in accordance with DMURS & TII Standards. 		
Does the proposed development constitute an EIA project? <i>(that is involving construction works or interventions in the natural surroundings)</i>		Yes	✓
		No	
If YES, Does the proposed development fall within a class of development set out in Part 1 or Part 2, Schedule 5 of the Planning and Development Regulations, 2001 (as amended)?			
Tick	Threshold	Comment	Result
Yes			

No	✓	Schedule 5, Part 2, Section 10 –below 2000m of roadway Urban development which would involve an area greater than 2 hectares in the case of a business district, 10 hectares in the case of other parts of a built-up area and 20 hectares elsewhere	The proposals for the construction of a cyclist and pedestrian route, kerbing, pavement renewal, and pedestrian facilities, falls below the respective thresholds.	EIA Pre-screening is not required.
CONCLUSION				
Development is not within Part 1 or Part 2, Schedule 5. No EIA/Screening is required.			The proposed development is not of the type, size or nature of development listed in Schedule 5 Of the Planning and Development regulations, therefore an EIA is not required in this instance.	
Development is within Part 1 or Part 2 and is greater than, equal to, or there is no threshold. EIA is required.				
Development is within Part 1 or Part 2 but is less than threshold. EIA Screening is Required.				

EIA Pre-screening prepared by:

John O Flaherty, BEng, M.Eng., C.Eng. M.I.E.I.
TOBIN Consulting Engineers



Date:

24th July, 2023

Appendix D – Appropriate Assessment Screening Report

**APPROPRIATE ASSESSMENT
SCREENING
IN ACCORDANCE WITH THE REQUIREMENTS OF
ARTICLE 6(3) OF THE EU HABITATS DIRECTIVE**

Donegal County Council

Ballyshannon to Bundoran, Active Travel Project

For: James Curran

Roads and Transportation Directorate

Donegal County Council

Date: 27 Jun 2023

Prepared by:

JESSICA DEVLIN

PROJECT MANAGEMENT & ENVIRONMENTAL SERVICES

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Report produced by:

Jessica Devlin, Project Management & Environmental Services, 5 Pheasant Park, Donegal Town, Co. Donegal, Ireland.

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1.0 Introduction

This Screening Report has been prepared by Jessica Devlin for Donegal County Council for the purpose of a Part 8 Planning Permission Application pertaining to the Ballyshannon to Bundoran, Active Travel Project, Co. Donegal, Ireland. This report has been compiled to provide the competent authority with adequate information to make an appropriate assessment of the Project under Article 6(3) of the Habitat Directives. It describes the proposed project and the receiving environment. The zone of likely influence will be identified and any Natura 2000 sites within that zone will be identified. Any possible negative direct or indirect impacts on the Qualifying Interests (QI) of the Natura 2000 sites will be identified and the significance of the impacts will be assessed. This report follows the methodology set out in the Assessment of plans and projects significantly affecting Natura 2000 sites: Methodological guidance on the provisions of Article 6(3) and (4), E.C., 2002.

1.1 Natura 2000 and Appropriate Assessment

The introduction of the EU Birds Directive and the Habitats Directive in 1979 and 1992 respectively, made member states legally obliged to establish a Natura 2000 network of sites of highest biodiversity importance for rare and threatened habitats and species. This comprises Special Areas of Conservation (SACs, including candidate SACs), and Special Protection Areas (SPAs, including proposed SPAs). SACs are selected for the conservation of Annex I habitats (including priority types which are in danger of disappearance) and Annex II species (other than birds). SPAs are selected for the conservation of Annex I birds and other regularly occurring migratory birds and their habitats. The annexed habitats and species for which each site is selected correspond to the qualifying interests of the sites; from these the conservation objectives of the site are derived.

Articles 6(3) and 6(4) of the Habitat Directive 92/43/EEC require an Appropriate Assessment of plans and projects to prevent significant adverse effects on Natura 2000 sites. The Assessment must determine whether the plan or project is likely to have significant effects on the site and whether these effects will adversely affect the integrity of the site in terms of its nature conservation objectives.

Article 6(3) of the Habitats Directive states that:

“Any plan or project not directly connected with or necessary to the management of the site but likely to have a significant effect thereon, either individually or in combination with other plans or projects, shall be subject to appropriate assessment of its implications for the site in view of the site's conservation objectives. In the light of the conclusions of the assessment of the implications for the site and subject to the provisions of paragraph 4, the competent national authorities shall agree to the plan or project only after having ascertained that it will not adversely affect the integrity of the site concerned and, if appropriate, after having obtained the opinion of the general public.”

The assessment can be broken down into 4 main stages:

Stage 1 - Screening: Results of preliminary impact identification and assessment of significance of impacts.

Stage 2 - Appropriate Assessment: Assessment of the impact on the integrity of the site(s) and assessment of mitigation measures (NIS Report).

Stage 3 - Assessment of alternative solutions.

Stage 4 - Imperative Reasons of Overriding Public Interest (IROPI): IROPI test and assessment of compensatory measures.

2.0 Statement of authority

Jessica graduated from the National University of Ireland, Galway in 1997 with a BSc. honours degree in Geology and obtained a MSc. in Applied Environmental Science from Queens University Belfast in 2001. She attained a National Certificate in Eco-Tourism, from Sligo Institute of Technology in 2005 and in 2014 completed Geographical Information Systems for Environmental Investigations, University College Dublin.

Over the years, Jessica has gained a wide range of experience in research, consultancy and project management with particular emphasis on sustainable development in freshwater, marine and coastal environments.

As field scientist with the Queens University Marine Station in Portaferry, Jessica carried out habitat surveys with respect to the decline of salmonid populations in Northern Ireland Rivers. She progressed to research assistant with Queens University and the Department of Agriculture & Rural Development. As project manager for the Donegal County Council - Marine & Water Leisure Programme, she managed projects on sustainable development of the marine leisure product. Jessica also worked with the University College Cork Coastal and Marine Research Centre in partnership with Donegal County Council and the University of Ulster, as manager of the Donegal element of a North West Europe Interreg Project called IMCORE (Innovative Management of Europe's Changing Coastal Resource). For the past 10 years Jessica has been self-employed working as a project manager and environmental consultant, specialising in freshwater, marine, coastal and environmental projects. Her client base is wide reaching from state agencies to community groups, individuals, angling clubs and private developers.

3.0 Methodology

- Liaison with James Curran and Michael Canning, Donegal County Council.
- Site visit and walkover survey on 22 June 2023.
- Desk research (list not exhaustive, see section 11 for full detail).
- Online data available on European sites and protected habitats/species as held by the National Parks and Wildlife Service (NPWS) from www.npws.ie, including conservation objectives documents.
- Online data available on protected species as held by the National Biodiversity Data Centre (NBDC) from www.biodiversityireland.ie.
- Information on www.catchments.ie and www.epa.ie with regard to water quality.
- Information on groundwater resources and groundwater quality in the area available from www.epa.ie and www.gsi.ie.

This report has been prepared using the following guidance. A full list of research sources and references can be seen in section 11.

- Dept. of Environment Heritage and Local Government (2009) Appropriate Assessment of plans and projects, Guidance for planning authorities.
- European Commission Environment DG (2001) Assessment of plans and projects significantly affecting Natura 2000 sites, Methodological guidance on the provisions of Article 6(3) and (4) of the Habitats Directive 92/43/EEC November 2001.
- OPR Practice Note (March 2021) Appropriate Assessment Screening for Development Management.

4.0 Overview of project proposals

The proposed Greenway on the R267 between Bundoran to Ballyshannon is approximately 4.5km. The scheme is situated on the north side of the N15. The Greenway will provide Active Travel connectivity between the two towns. The R267 has been identified by Donegal County Council for improvement given the current sub-standard alignment, cross section and pavement condition as well as the likely increase in pedestrian traffic associate with hotels, camping sites, golf course and several tourists attractions. The Proposed scheme will commence in Bundoran town and terminate in Ballyshannon, see figure 1.

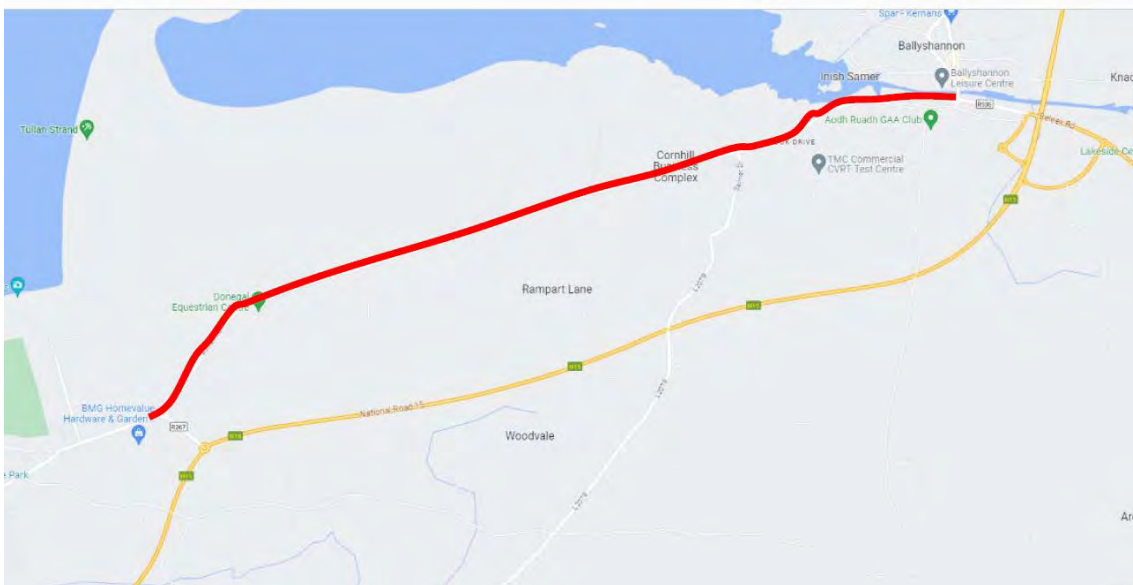


Figure 1. Outline view of location as supplied by Donegal County Council

5.0 Overview of Natura 2000 sites

5.1 Zone of influence

The approach to screening is likely to differ somewhat for plans and projects, depending on scale and on the likely effects and should include any Natura 2000 sites within the likely zone of impact of the plan or project. The zone of influence of a proposed development is the geographical area over which it could affect the receiving environment in a way that could have significant effects on the Qualifying Interests of a Natura 2000 site. This should be established on a case-by-case basis using

the Source-Pathway-Receptor framework and not by arbitrary distances (such as 15 km) (OPR, 2021).. see figure 2.

The Natura 2000 Sites have been assessed in terms of whether a Source - Pathway - Receptor relationship exists, and screened out accordingly. Where no source - pathway- receptor relationship is considered to exist these Natura 2000 sites are screened out and will not be discussed further in this report, see table 1.

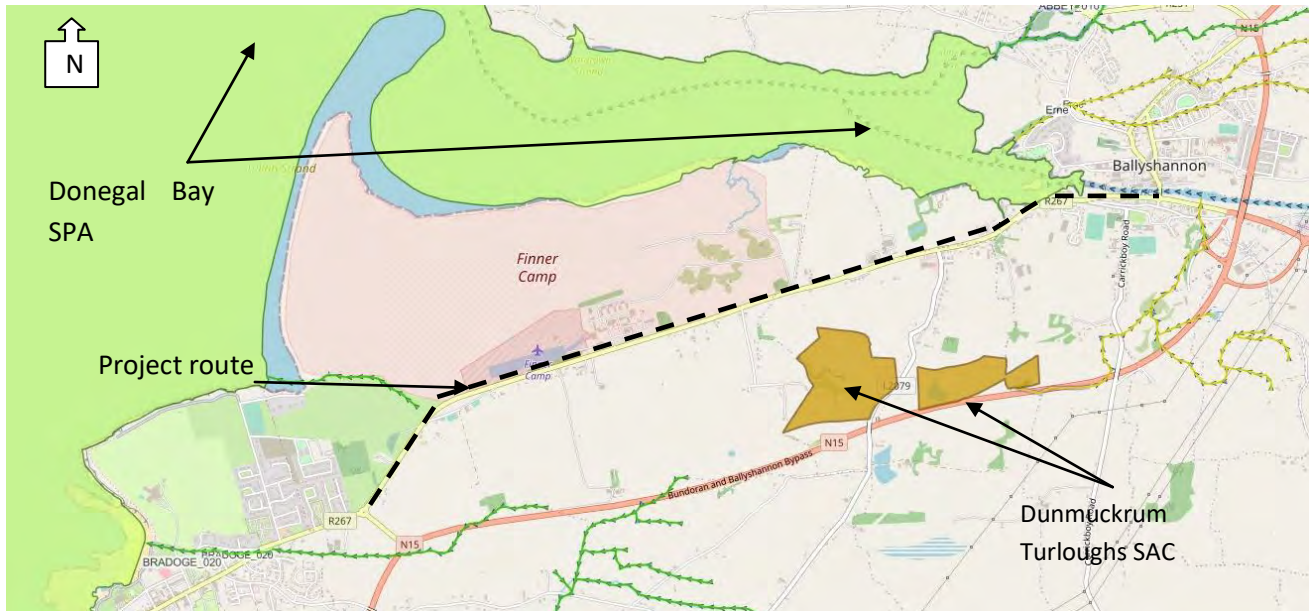


Figure 2. Project location in relation to Natura 2000 sites Donegal Bay SPA and Dunmuckrum Turloughs SAC, within the zone of influence of the project. River network and water quality in the Ballyshannon - Bundoran area. (Map source catchments.ie accessed 26 June 2023, © ESRI, © OSI)

Natura 2000 Site / (Site Code) / Distance from project (m)	Source Pathway Receptor Relationship Screened IN/ OUT
<p>Donegal Bay SPA (004151), project 20m from SPA boundary at closest point.</p> <p>Qualifying Interests: Great Northern Diver (<i>Gavia immer</i>) [A003] Light-bellied Brent Goose (<i>Branta bernicla hrota</i>) [A046] Common Scoter (<i>Melanitta nigra</i>) [A065] Sanderling (<i>Calidris alba</i>) [A144] Wetland and Waterbirds [A999]</p>	<p>Potential hydrological, acoustic and visual link with the SPA.</p> <p>Screened IN</p>
<p>Dunmuckrum Turloughs SAC (002303) (250m at closest point)</p> <p>3180 Turloughs* (* denotes a priority habitat)</p>	<p>Potential Source - Pathway - Receptor relationship. Pollution via road drainage could cause deterioration of groundwater quality and quantity.</p> <p>Screened IN</p>

Table 1. Initial screening of Natura 2000 sites within zone of influence.

The Natura 2000 sites have been considered in terms of the potential impacts the project may have on the features of interest and conservation objectives of the Natura 2000 sites. Dunmuckrum Turloughs SAC and Donegal Bay SPA are in close proximity to the development site and have been screened in for further assessment.

5.2 Summary of Natura 2000 sites

Detailed site synopses can be seen in Appendix 1.

Donegal Bay SPA is approximately 15 km along its north-east/ south-west axis, with a width of 3 km to over 8 km. It provides extensive habitat for waterfowl. The inner bay has numerous small, grassy islands and areas of salt marsh. It has a diversity of marine biotopes and supports a range of macroinvertebrates and bivalves. Much of the shoreline is rocky or stony which varies from well-developed littoral reefs to shingle or cobble beaches. Donegal Bay supports an excellent diversity of wintering water-birds, especially species associated with shallow bays, it is considered to be of high ornithological importance. Two species have populations of international importance (Great Northern Diver and Light-bellied Brent Goose) and a further two species have populations of national importance (Common Scoter and Sanderling).

The Dunmuckrum Turloughs SAC site is situated in a karstic area of south Donegal. It comprises a group of four small turloughs, which lie in a W-E line and are probably hydrologically connected. The westernmost Turlough is the largest and best developed. The base remains wet throughout the year with water amongst the marsh vegetation, but the sides dry out entirely and are covered by heathy grassland with outcropping limestone. Pools remain at the western end in summer and there is an obvious swallow hole with evidence of a seasonal stream. The second turlough in the series also has a permanent marsh at the base but the final two are smaller and dry out in summer. Typical turlough scrub woodland occurs at the flood line. For water quality reasons, areas of improved grassland are included in the site.

Conservation Objectives

The overall aim of the Habitats Directive is to maintain or restore the favourable conservation status of habitats and species of community interest. A site-specific conservation objective aims to define favourable conservation condition for a particular habitat or species at that site.

Favourable Conservation Status is defined by Articles 1(e) and 1(i) of the Habitats Directive as follows:

"The conservation status of a natural habitat is the sum of the influences acting on it and its typical species that may affect its long-term natural distribution, structure and functions as well as the long-term survival of its typical species. The conservation status of a natural habitat will be taken as favourable when:

- *its natural range and areas it covers within that range are stable or increasing; and*
- *the specific structure and functions which are necessary for its long-term maintenance exist and are likely to continue to exist for the foreseeable future; and*

- *the conservation status of its typical species is favourable’.*

The conservation status of a species is the sum of the influences acting on the species that may affect the long-term distribution and abundance of its populations. The conservation status will be taken as ‘favourable’ when:

- *the population dynamics data on the species concerned indicate that it is maintaining itself on a long-term basis as a viable component of its natural habitats; and*
- *the natural range of the species is neither being reduced nor is likely to be reduced for the foreseeable future; and*
- *there is, and will probably continue to be, a sufficiently large habitat to maintain its populations.”*

Conservation objectives for Donegal SPA (004151):

Objective 1: To maintain the favourable conservation condition of Great Northern Diver, Light-bellied Brent Goose, Common Scooter and Sanderling in Donegal Bay SPA Table 2. Conservation objective attributes and targets for water birds in Donegal Bay SPA

Objective 2: To maintain the favourable conservation condition of the wetland habitat in Donegal Bay SPA as a resource for the regularly-occurring migratory waterbirds that utilise it.

Conservation objectives for Dunmuckrum Turloughs SAC 002303

Objective 1: To maintain the favourable conservation condition of Dunmuckrum Turloughs SAC

5.3 Other plans/projects

5.3.1 The Wildlife Acts 1976 to 2021

The Wildlife Act is the principal national legislation providing for the protection of wildlife and the control of some activities that may have a negative effect on wildlife. The Wildlife (Amendment) Act 2000 strengthened the 1976 Act by, among other things, giving statutory protection to Natural Heritage Areas, improving existing measures to enhance protection of wildlife species and their habitats (e.g. fish and aquatic invertebrate species, hedgerow cutting) and strengthening the protective regime for Special Areas of Conservation (SACs).

The conservation of biodiversity in Ireland has been strengthened and expanded by EU law including the Water Framework Directive, the Birds Directive and the Habitats Directive.

5.3.2 County Donegal Development Plan 2018- 2024:

Policy NH-P-1 of the *County Donegal Development Plan 2018-2024* States the following:

“It is a policy of the Council to ensure that development proposals do not damage or destroy any sites of international or national importance, designated for their wildlife/habitat significance in

accordance with European and National legislation including: SACs, Special SPAs, NHAs, Ramsar Sites and Statutory Nature Reserves”

Any existing/proposed plan or project that could potentially affect Natura 2000 sites, in combination with the proposed development, must adhere to this environmental policy. Any projects or plans within the zone of influence of the project will be required to carry out Stage 1 and/or stage 2 of the Appropriate Assessment process thereby ensuring protection of Natura 2000 sites.

At the time of writing there were no live planning applications pending within the immediate vicinity of the project locations.

5.3.3 Water Framework Directive

The Water Framework Directive (WFD) obliges member states to manage their waters in an integrated and sustainable way. They must ensure that their waters achieve at least good status, generally by 2027 at the latest, and that current status doesn't deteriorate in any waters. To achieve good status and preserve the best waters, management plans have been prepared for districts around the country. Relevant projects underway as part of the implementation of this plan include:

Environmental Protection Agency (EPA) Monitoring Programme. The EPA is responsible for the monitoring of water quality around the country. Both chemical and ecological monitoring is undertaken by the EPA to ascertain water quality status.

5.3.4 International Union for the Conservation of Nature and Natural Resources (IUCN) Red Data Lists

IUCN Red Data Lists are a very important resource for conservation and protection of species and their habitats. Red Lists identify which species are in most danger, and categorise threatened species as follows: critically endangered (CR), endangered (EN), vulnerable (VU), near threatened (NT) or least concern (LC). Red lists are an internationally recognized system for highlighting species in danger.

5.3.5 Ramsar Sites

The Ramsar Convention is an international agreement for the conservation and wise use of wetlands. It is also known as the Convention on Wetlands and it is named after the city of Ramsar in Iran, where the Convention was signed in 1971. The Ramsar Convention (2010) defines wetlands as:

“ areas of marsh, fen, peatland or water, whether natural or artificial, permanent or temporary, with water that is static or flowing, fresh, brackish or salt, including areas of marine water the depth of which at low tide does not exceed six metres.”

6.0 Site description

A walkover survey of the sites was carried out on the 22 June 2023. This was a rapid assessment of the ecological features present, or potentially present, within a site and its surrounding area (the zone of influence) in relation to the project. The route was walked from Ballyshannon to Bundoran and the northern side surveyed, this was repeated in the opposite direction with the southern site surveyed.

The site is a linear site of 4.5km in length. It is along a busy road. In urban areas, in Ballyshannon, the route comprises hard ground and walls with a wide buffer between the existing infrastructure and the Erne River which flows to the sea east of Ballyshannon, see plates 1 and 2. The project is downstream of Kathleens Falls Hydro electric station. Mature trees occur along this section of the route, dominated by Sycamore. The route passes the historical Famine Departures Commercial Shipping Port, these buildings are in a derelict state of repair, with only the ground floor stone walls remaining. The route continues through the town and enters a rural environment after Portnason House where stone walls are bordered by grassy verges along the roads' gravel hard shoulder, see plate 4. This then changes to mature hedgerow until the fencing surrounding Finner Camp is reached, see plate 5. Just before the entrance to Finner camp the hard shoulder narrows (plate 6), a footpath occurs before and after the entrance to Finner Camp (plate 7), the hard shoulder then ends and a grassy verge forms the boundary between the road and Finner Camp (plate 8). A hard shoulder occurs along the road beside Donegal Equestrian Centre. There is a narrow grassy buffer between the road and the wetland to the north of the road and a tributary of the Bradogue River EPA ref _020_.

The route on the southern side is quite homogenous comprising hard shoulder, grassy verges and fencing in rural sections, with some derelict buildings and single dwellings. A hard shoulder and footpaths with stormwater drainage in urban sections dominate see plates 12 to 16.

6.1 Hydrology/ Hydrogeology

The beginning of the route follows the Erne River in Ballyshannon from the bridge, until the route veers in a more south westerly direction as it follows the road.

The road system surrounding the route has storm water drainage in the urban sections and percolates to ground in rural sections.

The Bradogue River passes under the road at the riding school and enters the sea some 800m away, see figure 2. At the time of survey this was completely dried up and appears to be a small tributary.

According the EPA website, www.catchments.ie, transitional waters of the Erne/Donegal Bay up to the bridge is classified as Good status. The Erne River in the wider catchment is classified as Moderate status. The water quality status of the Bradogue is Good, see figure 2. Groundwater is classified as Good.



Plate 1. Looking west from bridge towards Donegal Bay.



Plate 2. Looking west along existing footpath in Ballyshannon.

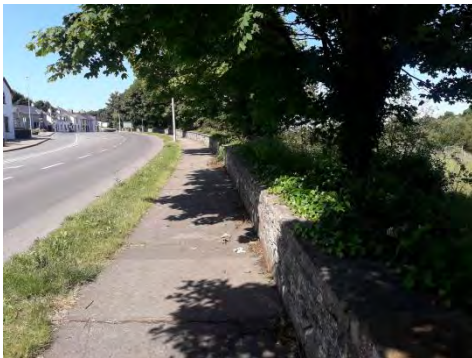


Plate 3. Looking south west along the R267.



Plate 4. Looking west towards Camphill Farm.



Plate 5. Looking west towards Finner Camp.



Plate 6. Looking west along Finner Camp, hard shoulder narrows, surface water percolates to ground.



Plate 7. Looking west towards approach to entrance of Finner Camp.



Plate 8. Looking west after entrance to Finner Camp, hard shoulder ends. Wire fence, mature sycamore trees and conifer woodland within Finner Camp grounds.



Plate 9. Looking west towards Donegal Bay, opposite Donegal Equestrian Centre, narrow grassy verge.



Plate 10. Looking north along Bradogue River (dried up) towards Donegal Bay, across wetland, with Yellow Iris dominating



Plate 11. Hard shoulder widens on approach to Bundoran, agricultural grassland in fields bordering the path some mature hedgerow and grassy verge.



Plate 12. Looking east from Bundoran roundabout (Finner) towards Ballyshannon.



Plate 13. Looking east grassy verge, fencing bordering agricultural land.



Plate 14. Looking east, opposite Finner Camp, single dwellings and private gardens.



Plate 15. Looking east on approach to Ballyshannon, grassy verge, hedgerow and surface water drainage percolating to ground.



Plate 16. Looking east entering Ballyshannon, stormwater drainage, built environment and footpaths.

7.0 Detailed project proposals as provided by Donegal County Council

Donegal County Council is applying for Planning Permission for the development of a path/cycleway along the R267 from Ballyshannon to Bundoran, Co. Donegal.

Given the speed differential and traffic volumes on the R267, a segregated cycle track is required. For speeds greater than 60kph, a cycle track behind a verge is recommended. Transport Infrastructure Ireland have details of cross section requirements to provide for cycle tracks in this manner in their publication DN-GEO-03031 Rural Road Link Design and DN-GEO-03036 Cross Sections and Headroom.

To provide for the development of a cycle track on this stretch of road, to maximise the efficiency in the use of space and to reduce traffic speeds to the speed limit, the cross section of the entire road must be considered.

A Type 3 single carriageway is the appropriate cross section for the road, given that it is no longer a national primary route. This informs the cross section of the other elements. Guidelines give a

desirable minimum of a 5m wide shared use two way cycle facility with pedestrians for the high volume of VRU traffic or 3m for low volume.

The Cycle track is located on Northern Side of R267 – There are a number of advantages to this option:

- It runs adjacent to Finner Camp where accesses are limited and therefore less conflict between VRUs and vehicles. (43 accesses on southern side between speed limits as opposed to 3 on northern side);*
- It is on the sea side of the road and views are better;*
- It removes the need to cross the link road to the N15 on the Bundoran roundabout;*
- It would link the main entrance to Finner Camp to the cycle track ;*
- It provides connectivity and uniformity of cycle lane provision right into the roundabout in Ballyshannon without the need for crossing facilities;*
- There are very few entrances to negotiate within the 60kph speed limit.*

The principal feature of the proposed development is the provision of a footpath/ cycleway along the northern side of the R267 to incorporate carriageway narrowing, tighter junction radii and formalised crossing facilities. Details are as follows:

Carriageways - Carriageway widths are reduced to 3.25m each side in accordance with DMURS.

Footpaths - A 5m footpath is to be provided on the north side of the R267

Crossing points - Raised table uncontrolled crossings in accordance with National Transport Authority's guidance may be provided inside the 60kmph speed limits.

Junction treatment – The crossing facilities in the rural and urban regions will be provided by TII standards and DMURS standards respectively. This may take the form of bend in or bend out facilities or raised tables.

Public Transport - Given the proximity of the site to the town, it is not felt that bus stop facilities are warranted.

Lighting - As part of the detailed design, a lighting design will be carried out to ensure adequacy of the current lighting arrangements and upgrade as necessary. Subject to agreement with the service provider, all cables will be installed underground.

Drainage – Longitudinal drainage will be required in the rural areas and it is envisaged to tie in with existing watercourses. In the Urban region, the existing gullies will be moved to tie in with the revised kerb locations and the existing drainage network will be used. Some over the edge drainage will also be employed.

Landscaping – Areas of landscaping and verge reinstatement may be required between the road and the cycleway and on any embankments formed in the rural aspect.

Road markings - Appropriate road and cycleway markings in accordance with the Traffic Signs Manual will be used throughout the scheme where required to clearly identify carriageways, priorities and turning movements. Junction road markings will be upgraded to reflect the change in layout.

Signage - Where required, existing mainline directional and route confirmatory signage will be relocated or altered to accommodate the required cycleway width. In accordance with TII and DMURS standards, signage will be kept to a minimum.

Services - Localised relocation of service poles or pillars may be required where these are located along the route of the proposed greenway. All works will be agreed and carried out in accordance with the requirements of the appropriate utility providers.

Methodology

The project will be retained within the existing road boundary and modifications to existing culverts throughout the scheme are not proposed. Where necessary the road will be realigned to the south by narrowing the hard shoulder on the south.

In the rural environs of the above project, filter drains will be used where appropriate for the longitudinal drainage. In the urban environs, the existing longitudinal drainage runs will be used with some kerb / gully relocations.

8.0 Assessment of project proposal in terms of potential direct, indirect or cumulative impacts on Natura 2000 Sites.

Table 2 explores where there may be potential for the project to impact Natura Sites and their qualifying interests.

Attribute	Description	Potential Impact to Natura 2000 site?
Size & Scale	4.5km linear project within existing road footprint.	No impact.
Land take	No land take	No impact.
Distance from the Natura 2000 site or key features of the site	20m from the boundary of Donegal SPA at closest point in Ballyshannon. 250m from Dunmuckrum Turloughs SAC at closest point.	Works are small scale and of short duration. Within footprint of the existing road. Some potential for run off during construction of path. Noise disturbance.
Resource requirements (water abstraction etc.)	None.	
Emissions (disposal to land, water, or air)	Noise emissions. Potential for run-off and pollution from sites (very low risk)	
Excavation requirements	Site preparation and resurfacing, extensive excavation or levelling site is not envisaged.	
Transportation requirements	Lorries and excavators for delivery and works	
Duration of construction, operation etc.	Short duration; completed in c.7 months No change in current use.	

Table 2. Project activity and the potential direct, indirect and cumulative impacts it may have.

9.0 Assessment of significance

As discussed in section 8, there is little potential for the project to impact on Donegal Bay SPA or Dunmuckrum Turloughs SAC.

Table 3 explores further the likely significance of the project and the potential impacts identified, in terms of disturbance to key species, habitat or species fragmentation, reduction in species density and changes in key indicators of conservation value, i.e. water quality.

9.1 Assessment of project proposal in terms of habitat loss, disturbance, fragmentation or reduction in species density:

Natura 2000 site & Qualifying Interest	Potential impacts from the proposed development on the integrity of the Natura 2000 site, individually or in combination with other projects	Significance of Impact
<p>Donegal Bay SPA (004151) Qualifying Interests: Great Northern Diver (<i>Gavia immer</i>) [A003] Light-bellied Brent Goose (<i>Branta bernicla hrota</i>) [A046] Common Scoter (<i>Melanitta nigra</i>) [A065] Sanderling (<i>Calidris alba</i>) [A144] Wetland and Waterbirds [A999]</p> <p>Dunmuckrum Turloughs SAC (02303)</p>	<p>Direct loss of habitat There will be no direct loss of habitat within the Natura 2000 sites.</p>	<p>No impact. No potential for significant effects.</p>
	<p>Indirect loss of habitat: A species may stop using a habitat due to increased disturbance or habitat degradation on site. Habitat degradation due to hydrological impacts via surface water.</p> <p>Water quality: Construction: The construction period of the proposed development poses little risk to the surrounding environment. The site is already developed, the project will be retained within the existing road boundary and modifications to existing culverts throughout the scheme are not proposed. Major excavation will therefore not be required; normal construction practices are sufficient to negate any potential risks.</p> <p>Operation: Cycleway in rural environment: Surface water will be treated through the permeation process. Excess surface water runoff will drain to filter drains. Cycleway in urban areas will continue to use the existing drainage system. Based on the information provided there is no significant risk of pollution from the proposal. Operation at the site will not change. Quality and quantities of surface and ground water will not change.</p>	<p>No impact. No potential for significant effect</p>
	<p>Disturbance / Displacement: Noise and visual disturbance: Waterfowl: Donegal Bay is designated for a number of <u>wintering</u> waterbirds. For waterbirds, construction-related disturbance effects would not be expected to extend beyond a distance of c. 300m, as noise levels associated with general construction activities would attenuate to close to background levels at that distance</p>	<p>No impact No potential for significant effects.</p>

Natura 2000 site & Qualifying Interest	Potential impacts from the proposed development on the integrity of the Natura 2000 site, individually or in combination with other projects	Significance of Impact
	<p>(Cutts <i>et al.</i>, 2009). These distances are applicable to the construction phase.</p> <p>The proposed development is within 20m of Donegal Bay SPA at its closest point; the project is linear and will be implemented along the route in sections. Works will therefore not be consistently within the 20m range of the SPA and are 800+m away from the SPA for the majority of the route. While the construction phase may create noise, it is unlikely to cause disturbance or displacement of bird species in Donegal Bay SPA for the following reasons:</p> <p>The Construction phase will be short and noise will not be consistently within range of the SPA.</p> <p>Works are screened from activity by the lay of the land and the natural and manmade screening, buildings and dunes along the shore line.</p> <p>The area of bay close to the project is small relative to Donegal Bay SPA</p> <p>There will be no significant change in use or activity at the site which is screened by the lay of the land and the natural and manmade topography.</p>	
	<p>Habitat or Species fragmentation</p> <p>There will be no habitat fragmentation. The project is outside Donegal SPA and Dunmuckrum Turloughs SAC, and will not encroach on supporting habitat for either site.</p>	<p>No Impact. No potential for significant effects.</p>
	<p>Reduction in Species Density</p> <p>It has been demonstrated in the discussion above in terms of hydrological impacts and displacement or disturbance there is no potential for a significant impact either during construction or operation.</p>	<p>No Impact. No potential for significant effects.</p>
	<p>In combination: At the time of writing there were no live planning applications pending within the immediate vicinity of the project locations. Recent planning permissions in the wider area including funfair extension in Bundoran, and septic tank installations near Tullaghan Roundabout and Bundoran Regeneration project, these are all small projects and have been screened out for Appropriate Assessment. Over the last 10 years planning applications granted within the project area have been extensions to existing buildings and change of use applications, the majority of which were for buildings in Ballyshannon, planning permission for a 70 bed nursing home has been granted at the roundabout at the end of the route in Bundoran.</p> <p>Any other future projects will be subject to Appropriate Assessment.</p>	<p>No cumulative effects anticipated.</p>

Table 3. Likely significance of impacts.

9.2 Cumulative Impacts

The potential for cumulative impacts to arise from the project proposal is regulated and controlled by the environmental policies and objectives of the Donegal County Council; policy NH-P-1 of the *County Donegal Development Plan 2018-2024* states the following:

“It is a policy of the Council to ensure that development proposals do not damage or destroy any sites of international or national importance, designated for their wildlife/habitat significance in accordance with European and National legislation including: SACs, Special SPAs, NHAs, Ramsar Sites and Statutory Nature Reserves”

Any existing/proposed plan or project that could potentially affect Natura 2000 sites, in combination with the proposed development, must adhere to the overarching environmental policies of the County Development Plan and Local Area Plans. These policies will ensure the protection of the Natura 2000 sites within the zone of influence of the proposed project and include the requirement for any future plans or projects to undergo Screening for Appropriate Assessment and/or Appropriate Assessment (NIS) to examine and assess their effects on Natura 2000 sites, alone and in combination with other plans and projects.

It has been demonstrated that there is no potential for significant effects on any Natura site, therefore cumulative effects are unlikely.

10.0 Conclusion

Donegal County Council is applying for planning permission for the development of a footpath / cycleway along the R267 from Ballyshannon to Bundoran on the northern side of the road. The project is a small scale, linear project spreading over a distance of 4.5km and within the existing road infrastructure.

Natura 2000 sites within the zone of influence of the project were assessed. The project is close to Donegal Bay SPA (004151) and Dunmuckrum Turloughs SAC (002303). A site visit and walkover survey was carried out on 22 June 2023 and a desk study completed using existing available data for the site.

The project proposal has been assessed in terms of the likely impacts the proposal may have on the Natura 2000 sites in the area. The significance of impacts identified (if any) has been determined. It has been determined that the project does not pose a risk to the marine and coastal environment or Donegal Bay SPA or Dunmuckrum Turloughs SAC.

This report presents a Stage 1 Appropriate Assessment Screening outlining the information required for the competent authority to screen for appropriate assessment and to determine whether or not the project, either alone or in combination with other plans and projects, in view of best scientific knowledge, is likely to have a significant effect on any Natura 2000 site.

The Competent Authority has been provided with information to conduct a Stage 1 Screening for Appropriate Assessment and consider whether, in view of best scientific knowledge and in view of the conservation objectives of the relevant Natura 2000 sites, the Project, individually or in combination with other plans or projects is likely to have a significant effect on any Natura 2000 site.

It can be objectively concluded that there is no possibility of significant impacts on any Natura 2000 site, their features of interest and site specific conservation objectives. Stage 2 of the Appropriate Assessment process (Natura Impact Statement) is not required.

11.0 References

The following research documents/ sources were used in the preparation of this report:

Cutts, N., Phelps, A. and Burdon, D. (2009) *Construction and Waterfowl: Defining Sensitivity, Response, Impacts and Guidance. Report to Huber INCA*. Institute of Estuarine and Coastal Studies, University of Hull.

Dept. of Environment Heritage and Local Government (2009) *Appropriate Assessment of plans and projects, Guidance for planning authorities*.

European Commission Environment DG (2001) *Assessment of plans and projects significantly affecting Natura 2000 sites*, Methodological guidance on the provisions of Article 6(3) and (4) of the Habitats Directive 92/43/EEC November 2001.

European Environment Agency (2012) *Report under the Article 17 of the Habitats Directive Period 2007-2012* European Topic Centre on Biological Diversity.

NPWS (2012a) Donegal Bay Special Protection Area (Site Code 4151) Conservation Objectives Supporting Document Version 1.

NPWS (2012b) *Conservation Objectives: Donegal Bay SPA 004151*. Version 1.0. National Parks and Wildlife Service, Department of Arts, Heritage and the Gaeltacht.

NPWS (2017a) Conservation objectives supporting document: Turloughs* and Rivers with muddy banks with *Chenopodium rubri* p.p. and *Bidens* p.p. vegetation

NPWS (2017b) Donegal Bay SPA Standard Data Form.

NPWS (2021) Conservation Objectives: Dunmuckrum Turloughs SAC 002303. Version 1. National Parks and Wildlife Service, Department of Housing, Local Government and Heritage.

OPR Practice Note (March 2021) *Appropriate Assessment Screening for Development Management*.

Online sources accessed 26 June 2023

www.birdwatchireland.ie

www.NPWS.ie

www.catchments.ie

www.GSI.ie

www.epa.ie

www.biodiversityireland.ie

Appendix 1: Site Synopses

SITE NAME: DONEGAL BAY SPA, SITE CODE: 004151

Donegal Bay SPA is a very large, marine-dominated, site. It extends from Doorin Point, to the west of Donegal Town, to Tullaghan Point in County Leitrim, a distance of approximately 15 km along its north-east/south-west axis. It varies in width from about 3 km to over 8 km. The site includes the estuary of the River Eske, which flows through Donegal Town, and the estuary of the River Erne, which flows through Ballyshannon. Much of the shoreline is rocky or stony, with well-developed littoral reefs in places. There are also extensive stretches of sandy beaches, especially from the Murvagh peninsula southwards to Rosstown and at the outer part of the estuary of the River Erne. Shingle or cobble beaches are also represented. There are extensive areas of intertidal flats associated with the estuary of the River Eske, reflecting the very sheltered conditions in this part of the bay. These have been shown to be biotope rich, and supporting a range of macro-invertebrates, including polychaete worms (*Hediste diversicolor*, *Arenicola marina* and *Nephtys hombergii*) and bivalves (*Scrobicularia plana*, *Cerastoderma edule* and *Macoma balthica*).

Elsewhere, a narrow fringe of intertidal flats is exposed at low tides. Salt marshes are found in the sheltered conditions of the innermost part of the bay. A number of small, grassy, islands occur in the innermost part of the bay. The waters of the shallow bay overlie mostly sandy substrates, though reefs occur in places.

The site is a Special Protection Area (SPA) under the E.U. Birds Directive, of special conservation interest for the following species: Great Northern Diver, Light-bellied Brent Goose, Common Scoter and Sanderling. The E.U. Birds Directive pays particular attention to wetlands, and as these form part of this SPA, the site and its associated waterbirds are of special conservation interest for Wetland & Waterbirds.

Donegal Bay supports an excellent diversity of wintering waterbirds, especially species associated with shallow bays (all figures are mean peak counts for four of the five winters between 1995/96 and 1999/2000). It has an internationally important wintering population of Great Northern Diver (138) and is consistently one of the top sites in the country for this species. It also has one of the few regular populations of Black-throated Diver in the country (11), and Red-throated Diver (21). It supports an internationally important population of Light-bellied Brent Goose (207) and nationally important populations of Common Scoter (860) and Sanderling (68). A range of other species associated with estuarine and shoreline habitats occurs at the site, including Cormorant (29), Shelduck (24), Wigeon (224), Mallard (100), Long-tailed Duck (14), Red-breasted Merganser (38), Oystercatcher (581), Ringed Plover (99), Golden Plover (103), Lapwing (122), Dunlin (269), Bar-tailed Godwit (49), Curlew (359), Redshank (93), Greenshank (12) and Turnstone (53). Gulls are regular in autumn and winter, especially Black-headed Gull (239) and Common Gull (297). This large coastal site is of high ornithological importance, with two species having populations of international importance (Great Northern Diver and Light-bellied Brent Goose) and a further two species having populations of national importance (Common Scoter and Sanderling). Also of note is that five of the regularly occurring species are listed on Annex I of the E.U. Birds Directive, i.e. Great Northern Diver, Black-throated Diver, Red-throated Diver, Golden Plover and Bar-tailed Godwit.

NPWs version date 13.10.2010

SITE NAME: DUNMUCKRUM TURLOUGHES SAC, SITE CODE: 002303

This site is located about 2 km south-west of Ballyshannon in Co. Donegal. It consists of a series of low-lying winter-flooded depressions set in an undulating landscape of limestone hills.

The site is a Special Area of Conservation (SAC) selected for the following habitats and/or species listed on Annex I / II of the E.U. Habitats Directive (* = priority; numbers in brackets are Natura 2000 codes):

[3180] Turloughs*

The most westerly of the depressions, in Lugnav townland, supports typical turlough vegetation, reflecting the zonation caused by periodic flooding. The wettest areas at the base contain small patches of open water surrounded by marsh plants. At the eastern end there is some stonewort (*Chara* sp.), Horned Pondweed (*Zannichellia palustris*), Common Duckweed (*Lemna minor*) and the moss *Fontinalis antipyretica*. The western pools are distinguished by the presence of Thread-leaved Water-crowfoot (*Ranunculus trichophyllus*), Blue Water-speedwell (*Veronica anagallis-aquatica*), Water-cress (*Nasturtium officinale*) and Lesser Marshwort (*Apium inundatum*). Exposed muds behind these areas support Equal-leaved Knotgrass (*Polygonum arenastrum*), Shepherd's-purse (*Capsella bursa-pastoris*), Marsh Yellow-cress (*Rorippa palustris*) and Marsh Cudweed (*Gnaphalium uliginosum*), amongst others.

A permanently wet area of scraw (i.e. floating) vegetation at the lowest part of this turlough is dominated by Bottle Sedge (*Carex rostrata*), Bogbean (*Menyanthes trifoliata*) and Common Spike-rush (*Eleocharis palustris*). This grades into a wet, sedge-dominated sward, with species such as Common Sedge (*Carex nigra*), Brown Sedge (*C. disticha*) and a wide range of wetland herbs. At the upper levels of flooding the vegetation grades into a leached limestone grassland, with patches of Hawthorn (*Crataegus monogyna*) and Blackthorn (*Prunus spinosa*) scrub woodland. Notable species here include Adder's-tongue (*Ophioglossum vulgatum*), Grass-of-parnassus (*Parnassia palustris*) and an abundance of the moss *Cinclidotus fontinaloides* on all the inundated rocky surfaces.

Further east, in Dunmuckrum townland, lies a smaller turlough basin with a permanent marsh at the base, mostly dominated by sedges (*Carex nigra* and *C. rostrata*) and Meadowsweet (*Filipendula ulmaria*). A shorter grazed grassland vegetation surrounds this with Creeping Bent (*Agrostis stolonifera*), Jointed Rush (*Juncus articulatus*) and Autumn Hawkbit (*Leontodon autumnalis*), amongst others. Two further depressions lie just to the east, straddling the disused railway line. Although seasonally flooded, these basins are smaller and drier, and support mainly grassland vegetation dominated by Creeping Bent.

The site is of conservation importance as it represents the mostly northerly turlough known in the country, if not globally. Turlough habitat is listed with priority status in Annex I of the E.U. Habitats Directive.

Version date: 6.01.2014

Appendix E – Planning Notice

SITE NOTICE

PLANNING AND DEVELOPMENT ACT 2000 (as amended)
PLANNING AND DEVELOPMENT REGULATIONS 2001 (as amended)
ARTICLE 81

NOTICE PURSUANT TO ARTICLE 81, PART 8 OF THE ABOVE REGULATIONS, RELATING TO A PROPOSED DEVELOPMENT BY DONEGAL COUNTY COUNCIL

Take Notice that Donegal County Council proposes to carry out the following development adjacent to the existing R267 Finner Road between Ballyshannon and Bundoran located in the Donegal Municipal District as detailed in the schedule below:

SCHEDULE OF PROPOSED WORK

No.	Title	Townlands	Local Roads Office for Viewing Plans
1	Ballyshannon to Bundoran Active Travel Scheme	Magheracar, Drumacrin, Finner, Dunmuckrum and Carrickboy	Online at: www.donegalcoco.ie Services > Roads Service > Roads Service Public Consultation Drumlonagher, Donegal Town, Co. Donegal, F94 DK6C Lifford Public Service Centre, County House, The Diamond, Lifford, Co. Donegal, F93 Y622
Description and Extents			
Donegal County Council proposes to provide a new shared use pedestrian and cyclist facility adjacent to the existing R267 Ballyshannon to Bundoran road (Finner Road) in accordance with the Part 8 application. The project will include:			
<ul style="list-style-type: none"> a) Provision of a shared use cycle / pedestrian facility on the northern verge of the existing R267 (Finner Road) b) Provision of appropriate road crossings both controlled and uncontrolled c) Provision of upgraded public street lighting within the urban scheme extents. d) Provision of all appropriate drainage, earthworks, road marking and road signage for the project. e) Minor localised realignment of the existing R267 Regional Road to the south to accommodate the new facility. f) All other ancillary and tie in works to TII, DMURS and National Cycle Manual standards. 			

The plans and particulars for this proposed development will be available for inspection at the following locations:

- Drumlonagher, Donegal Town, Co. Donegal, F94 DK6C
- Lifford Public Service Centre, County House, The Diamond, Lifford, Co. Donegal, F93 Y622
- Plans and particulars for this proposed development will also be available for inspection or download at www.donegalcoco.ie under section "Services > Roads Service > Roads Service Public Consultation".

The proposals will be available for inspection from Friday 4th August 2023 until 4pm on Tuesday, 5th September 2023.

Submissions and observations with respect to the proposed development, dealing with the proper planning and development of the area in which the development is situated may be made in writing to The County Secretariat Office, Donegal County Council, Lifford, County Donegal or emailed to activetravel@donegalcoco.ie before **4.00pm on Thursday 21st September 2023**.

Note that in accordance with Article 120(1)(b)(i) of the Planning and Development Regulations 2001 (as amended) Donegal County Council has concluded, based on a preliminary examination of the nature, size, and location of the development, that an Environmental Impact Assessment (EIA) is not required.

Please mark the front of the envelope with the project name as per the above Schedule Title.

**Bryan Cannon,
A/Director of Service,
Roads & Transportation,
Donegal County Council,
County House,
Lifford.**



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